

SCIP #1  
GRANT

APPLICATION FOR FINANCIAL ASSISTANCE  
Revised 4/99

**IMPORTANT:** Please consult the "Instructions for Completing the Project Application" for assistance in completion of this form.

SUBDIVISION: : Colerain Township CODE# 061-16616

DISTRICT NUMBER: 2 COUNTY Hamilton DATE 8 / 31 / 2008

CONTACT: Bruce E. McClain PHONE # (513) 385 - 7502

(THE PROJECT CONTACT PERSON SHOULD BE THE INDIVIDUAL WHO WILL BE AVAILABLE ON A DAY-TO-DAY BASIS DURING THE APPLICATION REVIEW AND SELECTION PROCESS AND WHO CAN BEST ANSWER OR COORDINATE THE RESPONSE TO QUESTIONS)

FAX (513) 245-6163 E-MAIL bmcclain@coleraintwp.org

PROJECT NAME: Belhaven Drive and Flamingo Lane Reconstruction

SUBDIVISION TYPE

(Check Only 1)

- ☐ 1. County  
☐ 2. City  
☒ 3. Township  
☐ 4. Village  
☐ 5. Water/Sanitary District  
(Section 6119 O.R.C.)

FUNDING TYPE REQUESTED

(Check All Requested & Enter Amount)

- ☒ 1. Grant \$ 282,590  
☐ 2. Loan \$ \_\_\_\_\_  
☐ 3. Loan Assistance \$ \_\_\_\_\_

PROJECT TYPE

(Check Largest Component)

- ☒ 1. Road  
☐ 2. Bridge/Culvert  
☐ 3. Water Supply  
☐ 4. Wastewater  
☐ 5. Solid Waste  
☐ 6. Stormwater

TOTAL PROJECT COST: \$ 403,700

FUNDING REQUESTED: \$ 282,590

DISTRICT RECOMMENDATION  
To be completed by the District Committee ONLY

GRANT: \$ 282,590 LOAN ASSISTANCE: \$ \_\_\_\_\_  
SCIP LOAN: \$ \_\_\_\_\_ RATE: \_\_\_\_\_ % TERM: \_\_\_\_\_ yrs.  
RLP LOAN: \$ \_\_\_\_\_ RATE: \_\_\_\_\_ % TERM: \_\_\_\_\_ yrs.

(Check Only 1)

- ☒ State Capital Improvement Program ☐ Small Government Program  
☐ Local Transportation Improvements Program

2008 SEP 12 AM 10:08  
OFFICE OF NEW BURLINGTON  
COUNTY ENGINEER

FOR OPWC USE ONLY

PROJECT NUMBER: C \_\_\_\_\_ / C \_\_\_\_\_  
Local Participation \_\_\_\_\_ %  
OPWC Participation \_\_\_\_\_ %  
Project Release Date: \_\_\_\_ / \_\_\_\_ / \_\_\_\_  
OPWC Approval: \_\_\_\_\_

APPROVED FUNDING: \$ \_\_\_\_\_  
Loan Interest Rate: \_\_\_\_\_ %  
Loan Term: \_\_\_\_\_ years  
Maturity Date: \_\_\_\_\_  
Date Approved: \_\_\_\_ / \_\_\_\_ / \_\_\_\_  
SCIP Loan \_\_\_\_\_ RLP Loan \_\_\_\_\_

**1.0 PROJECT FINANCIAL INFORMATION**

**1.1 PROJECT ESTIMATED COSTS:**  
(Round to Nearest Dollar)

**TOTAL DOLLARS**

**FORCE ACCOUNT  
DOLLARS**

- a.) **Basic Engineering Services:** \$     N/A    .00
- Preliminary Design      \$     N/A    .00  
    Final Design            \$     N/A    .00  
    Bidding                 \$     N/A    .00  
    Construction Phase     \$     N/A    .00
- Additional Engineering Services      \$     N/A    .00  
    \*Identify services and costs below.
- b.) **Acquisition Expenses:**  
    Land and/or Right-of-Way      \$     N/A    .00
- c.) **Construction Costs:** \$   282,590.00
- d.) **Equipment Purchased Directly:** \$     N/A    .00
- e.) **Permits, Advertising, Legal:** \$     N/A    .00  
    (Or Interest Costs for Loan Assistance  
    Applications Only)
- f.) **Construction Contingencies:** \$   121,110.00
- g.) **TOTAL ESTIMATED COSTS:** \$   403,700.00

\*List Additional Engineering Services here:  
Service:

Cost:

**1.2 PROJECT FINANCIAL RESOURCES:**  
(Round to Nearest Dollar and Percent)

	DOLLARS	%
a.) Local In-Kind Contributions	\$ <u>N/A .00</u>	<u>      </u>
b.) Local Revenues	\$ <u>121,110.00</u>	<u>30%</u>
c.) Other Public Revenues	\$ <u>N/A .00</u>	<u>      </u>
ODOT	\$ <u>N/A .00</u>	<u>      </u>
Rural Development	\$ <u>N/A .00</u>	<u>      </u>
OEPA	\$ <u>N/A .00</u>	<u>      </u>
OWDA	\$ <u>N/A .00</u>	<u>      </u>
CDBG	\$ <u>N/A .00</u>	<u>      </u>
OTHER _____	\$ <u>N/A .00</u>	<u>      </u>
<b>SUBTOTAL LOCAL RESOURCES:</b>	<b>\$ <u>121,110.00</u></b>	<b><u>30%</u></b>
d.) OPWC Funds		
1. Grant	\$ <u>282,590.00</u>	<u>70%</u>
2. Loan	\$ <u>N/A .00</u>	<u>      </u>
3. Loan Assistance	\$ <u>N/A .00</u>	<u>      </u>
<b>SUBTOTAL OPWC RESOURCES:</b>	<b>\$ <u>282,590.00</u></b>	<b><u>70%</u></b>
e.) <b>TOTAL FINANCIAL RESOURCES:</b>	<b>\$ <u>403,700.00</u></b>	<b><u>100%</u></b>

**1.3 AVAILABILITY OF LOCAL FUNDS:**

Attach a statement signed by the Chief Financial Officer listed in section 5.2 certifying all local share funds required for the project will be available on or before the earliest date listed in the Project Schedule section. (SEE ATTACHMENT "E")

ODOT PID# \_\_\_\_\_ Sale Date:  
STATUS: (Check one)  
    Traditional  
    Local Planning Agency (LPA)  
    State Infrastructure Bank

## 2.0 PROJECT INFORMATION

If project is multi-jurisdictional, information must be consolidated in this section.

### 2.1 PROJECT NAME: Belhaven Drive and Flamingo Lane Reconstruction

### 2.2 BRIEF PROJECT DESCRIPTION - (Sections A through C):

**A: SPECIFIC LOCATION:** From the intersection of Galbraith Road and Pippin Road, then north on Pippin Road one block then west onto Dolphin Drive then north on Haskell Drive approximately one block then west on Belhaven Drive. See location map.

**PROJECT ZIP CODE: 45239**

**B: PROJECT COMPONENTS:** The project components are as follows:

- 1) Remove existing concrete surface and curbs
- 2) Undercut subgrade as necessary
- 3) Catch basin reconstruction, repair or new
- 4) Install new concrete sidewalks and driveway aprons
- 5) Install new concrete curbs
- 6) Construct new curb ramps
- 7) Underdrain/edgedrain
- 8) Adjust catch basins, manholes, waterworks items, etc. as necessary
- 9) Storm line replacement and new installation
- 10) Pavement Fabric
- 11) Install bituminous aggregate base material
- 12) Install new asphaltic concrete surface
- 13) Reclamite
- 14) Seeding and mulching as necessary

### **C: PHYSICAL DIMENSIONS / CHARACTERISTICS:**

This street is 25' back to back of curb that is 49 years old. This street is concrete with concrete curb and gutter, the base has failed throughout. There are areas with voided pavement and sinkholes. Curbs are faulted and badly deteriorated. The joints are heaved, there are potholes and patches throughout. Water stands on the pavement adding to the deterioration. There are deteriorated storm lines in need of replacement – see attached TV reports. The overall pavement is in very poor condition and the rideability is very rough and bumpy. Our pavement management program, Micro Paver, rates this street with a failed condition rating – see attached Micro Paver report. This street needs to be reconstructed. Please see attachment “A” for project dimensions.

### **D: DESIGN SERVICE CAPACITY:**

Detail current service capacity vs. proposed service level.

Road or Bridge: Current ADT 125 Year: 2008 Projected ADT: 130 Year: 2009

Water/Wastewater: Based on monthly usage of 7,756 gallons per household, attach current rate ordinance. Current Residential Rate: \$ \_\_\_\_\_ Proposed Rate: \$ \_\_\_\_\_

Stormwater: Number of households served: \_\_\_\_\_

### 2.3 USEFUL LIFE / COST ESTIMATE: Project Useful Life: 20 Years.

Attach Registered Professional Engineer's statement, with original seal and signature confirming the project's useful life indicated above and estimated cost.

### 3.0 REPAIR/REPLACEMENT or NEW/EXPANSION:

TOTAL PORTION OF PROJECT REPAIR/REPLACEMENT \$ 403,700.00

TOTAL PORTION OF PROJECT NEW/EXPANSION \$ .00

### 4.0 PROJECT SCHEDULE: \*

	BEGIN DATE	END DATE
4.1 Engineering/Design:	<u>11 / 1 / 08</u>	<u>7 / 30 / 09</u>
4.2 Bid Advertisement:	<u>11 / 15 / 09</u>	<u>12 / 15 / 09</u>
4.3 Construction:	<u>3 / 1 / 10</u>	<u>12 / 31 / 10</u>
4.4 Right-of-Way/Land Acquisition:	<u>/N/A /</u>	<u>/N/A /</u>

\* Failure to meet project schedule may result in termination of agreement for approved projects. Modification of dates must be requested in writing by the CEO of record and approved by the commission once the Project Agreement has been executed. The project schedule should be planned around receiving a Project Agreement on or about July 1st.

### 5.0 APPLICANT INFORMATION:

#### 5.1 CHIEF EXECUTIVE

OFFICER David L. Foglesong  
TITLE Administrator  
STREET 4200 Springdale Road  
CITY/ZIP Colerain Township, Ohio 45251  
PHONE (513) 385 - 7500  
FAX (513) 245 - 6503  
E-MAIL dfoglesong@coleraintwp.org

#### 5.2 CHIEF FINANCIAL

OFFICER Heather E. Harlow  
TITLE Fiscal Officer Colerain Township  
STREET 4200 Springdale Road  
CITY/ZIP Colerain Township, Ohio 45251  
PHONE (513) 385 - 7500  
FAX (513) 245 - 6503  
E-MAIL hharlow@coleraintwp.org

#### 5.3 PROJECT MANAGER

Bruce E. McClain  
TITLE Road Superintendent  
STREET 4160 Springdale Road  
CITY/ZIP Colerain Township, Ohio 45251  
PHONE (513) 385 - 7502  
FAX (513) 245 - 6163  
E-MAIL bmcclain@coleraintwp.org

**Changes in Project Officials must be submitted in writing from the CEO.**

## 6.0 ATTACHMENTS/COMPLETENESS REVIEW:

Confirm in the blocks [ ] below that each item listed is attached.

- ☒ [X] A certified copy of the legislation by the governing body of the applicant authorizing a designated official to sign and submit this application and execute contracts. This individual should sign under 7.0, Applicant Certification, below.
- ☒ [X] A certification signed by the applicant's chief financial officer stating all local share funds required for the project will be available on or before the dates listed in the Project Schedule section. If the application involves a request for loan (RLP or SCIP), a certification signed by the CFO which identifies a specific revenue source for repaying the loan also must be attached. Both certifications can be accomplished in the same letter.
- ☒ [X] A registered professional engineer's detailed cost estimate and useful life statement, as required in 164-1-13, 164-1-14, and 164-1-16 of the Ohio Administrative Code. Estimates shall contain an engineer's original seal or stamp and signature.
- ☐ [ ] A cooperation agreement (if the project involves more than one subdivision or district) which identifies the fiscal and administrative responsibilities of each participant.
- ☐ [ ] Projects which include new and expansion components and potentially affect productive farmland should include a statement evaluating the potential impact. If there is a potential impact, the Governor's Executive Order 98-VII and the OPWC Farmland Preservation Review Advisory apply.
- ☐ [ ] Capital Improvements Report: (Required by O.R.C. Chapter 164.06 on standard form)
- ☒ [X] Supporting Documentation: Materials such as additional project description, photographs, economic impact (temporary and/or full time jobs likely to be created as a result of the project), accident reports, impact on school zones, and other information to assist your district committee in ranking your project. Be sure to include supplements which may be required by your *local* District Public Works Integrating Committee.

## 7.0 APPLICANT CERTIFICATION:

The undersigned certifies that: (1) he/she is legally authorized to request and accept financial assistance from the Ohio Public Works Commission; (2) to the best of his/her knowledge and belief, all representations that are part of this application are true and correct; (3) all official documents and commitments of the applicant that are part of this application have been duly authorized by the governing body of the applicant; and, (4) should the requested financial assistance be provided, that in the execution of this project, the applicant will comply with all assurances required by Ohio Law, including those involving Buy Ohio and prevailing wages.

Applicant certifies that physical construction on the project as defined in the application has NOT begun, and will not begin until a Project Agreement on this project has been executed with the Ohio Public Works Commission. Action to the contrary will result in termination of the agreement and withdrawal of Ohio Public Works Commission funding of the project.

David L. Foglesong, Administrator Colerain Township  
Certifying Representative (Type or Print Name and Title)

  
Signature/Date Signed

8/29/08

PROJECT: Belhaven Drive and Flamingo Lane Reconstruction  
ENG. EST.: \$403,700  
PREPARED BY: Colerain Township Public Works Department

# ENGINEER'S ESTIMATE

REF. NO.	ITEM NO.	DESCRIPTION	UNIT	QUANT.	UNIT \$	TOTAL
1	201	CLEARING AND GRUBBING	LS	1	\$ 5,000.00	\$ 5,000.00
2	202	ASPHALT/CONCRETE PAVEMENT REMOVED	SY	2,140	\$ 8.00	\$ 17,120.00
3	202	CONCRETE DRIVE REMOVED	SY	340	\$ 8.00	\$ 2,720.00
4	202	PIPE REMOVED	LF	520	\$ 10.00	\$ 5,200.00
5	202	CURB AND GUTTER REMOVED	LF	1,560	\$ 10.00	\$ 15,600.00
6	202	CATCH BASIN REMOVED	EA	6	\$ 250.00	\$ 1,500.00
7	202	CONC. WALK REMOVED, AS DIRECT. BY ENG.	SF	5,220	\$ 5.00	\$ 26,100.00
8	203	EXCAVA. N/INCLUDE. EMBANK.	CY	1,000	\$ 12.00	\$ 12,000.00
9	203	EMBANKMENT CONSTRUCTION	CY	100	\$ 12.00	\$ 1,200.00
10	203	SUBGRADE COMPACTION	SY	2,383	\$ 1.00	\$ 2,383.00
11	301	BITUMINOUS AGGREGATE BASE, pg 64-22	CY	244	\$ 150.00	\$ 36,600.00
12	304	AGGREGATE BASE, AS DIRECTED BY ENG.	CY	300	\$ 40.00	\$ 12,000.00
13	448	ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2, pg 64-22	CY	76	\$ 150.00	\$ 11,400.00
14	448	ASPHALT CONCRETE SURFACE COURSE TYPE 1, pg 64-22	CY	76	\$ 150.00	\$ 11,400.00
15	452	P.P. CEMENT CONC. PAVEMENT (7" DRIVES)	SY	335	\$ 35.00	\$ 11,725.00
16	603	3" CONDUIT, TYPE E, PVC & COUPLINGS	LF	120	\$ 20.00	\$ 2,400.00
17	603	12" CONDUIT, TYPE B, 706.02, CLIV	LF	520	\$ 75.00	\$ 39,000.00
18	604	MH-3 MH W/ FLAT SLAB TOP	EA	1	\$ 1,500.00	\$ 1,500.00
19	604	MODIFY & ADJ. WATER VALVE TO GRADE	EA	6	\$ 750.00	\$ 4,500.00
20	604	CATCH BASIN, CB-3	EA	5	\$ 1,500.00	\$ 7,500.00
21	604	SAN. MANHOLE ADJ. TO GRADE	EA	5	\$ 750.00	\$ 3,750.00
22	605	UNCLASSIFIED PIPE UNDERDRAIN, as directed by eng.	LF	1,560	\$ 20.00	\$ 31,200.00
23	608	5" CONCRETE WALK	SF	5,220	\$ 5.00	\$ 26,100.00
24	608	CURB RAMPS, TYPE 1	EA	4	\$ 600.00	\$ 2,400.00
25	609	CONCRETE CURB, TYPE 6	LF	1,560	\$ 10.00	\$ 15,600.00
26	614	MAINTAINING TRAFFIC	LS	1	\$ 5,000.00	\$ 5,000.00
27	623	CONSTRUCTION LAYOUT STAKES	LS	1	\$ 5,000.00	\$ 5,000.00
28	659	SEEDING AND MULCHING	SY	900	\$ 5.00	\$ 4,500.00
29	SPL	UNDERCUTTING	CY	500	\$ 55.00	\$ 27,500.00
30	SPL	RECLAMITE	SY	1,760	\$ 0.80	\$ 1,408.00
31	SPL	PAVEMENT FABRIC	SY	1,760	\$ 1.15	\$ 2,024.00
32	SPL	CINCINNATI WATER WORKS ITEMS	LS	1	\$ 12,000.00	\$ 12,000.00
33	SPL	SUPPLEMENTAL ITEMS	LS	1	\$ 40,370.00	\$ 40,370.00
					<b>TOTAL</b>	<b>\$ 403,700.00</b>

USEFUL LIFE: This is to certify that upon satisfactory completion of this work, the useful life of the streets on this project will be at least 20 years.

Signed:

  
WILLIAM BRAYSHAW

P.E.





# Colerain Township

**Trustees**  
BERNARD A. FIEDELDEY JR.  
JEFFREY F. RITTER  
JOSEPH R. WOLTERMAN  
**Fiscal Officer**  
HEATHER E. HARLOW  
**Administrator**  
DAVID L. FOGLESONG

**PUBLIC WORKS DEPARTMENT, ROAD DIVISION**  
**Bruce McClain, Road Superintendent • Tim Lange, Road Supervisor**  
4160 Springdale Road • Colerain Township, Ohio 45251-1834  
(513) 385-7502 • FAX (513) 245-6163 • [www.coleraintwp.org](http://www.coleraintwp.org)

August 29, 2008

## STATUS OF FUNDS REPORT

ATTACHMENT   E  

Project:   Belhaven Drive and Flamingo Lane Reconstruction  

This is to certify that the sum of \$121,110.00 Is available as the local matching funds in connections with Colerain Townships' application for State Capital Improvement Program (SCIP) Funds for the above mentioned project.

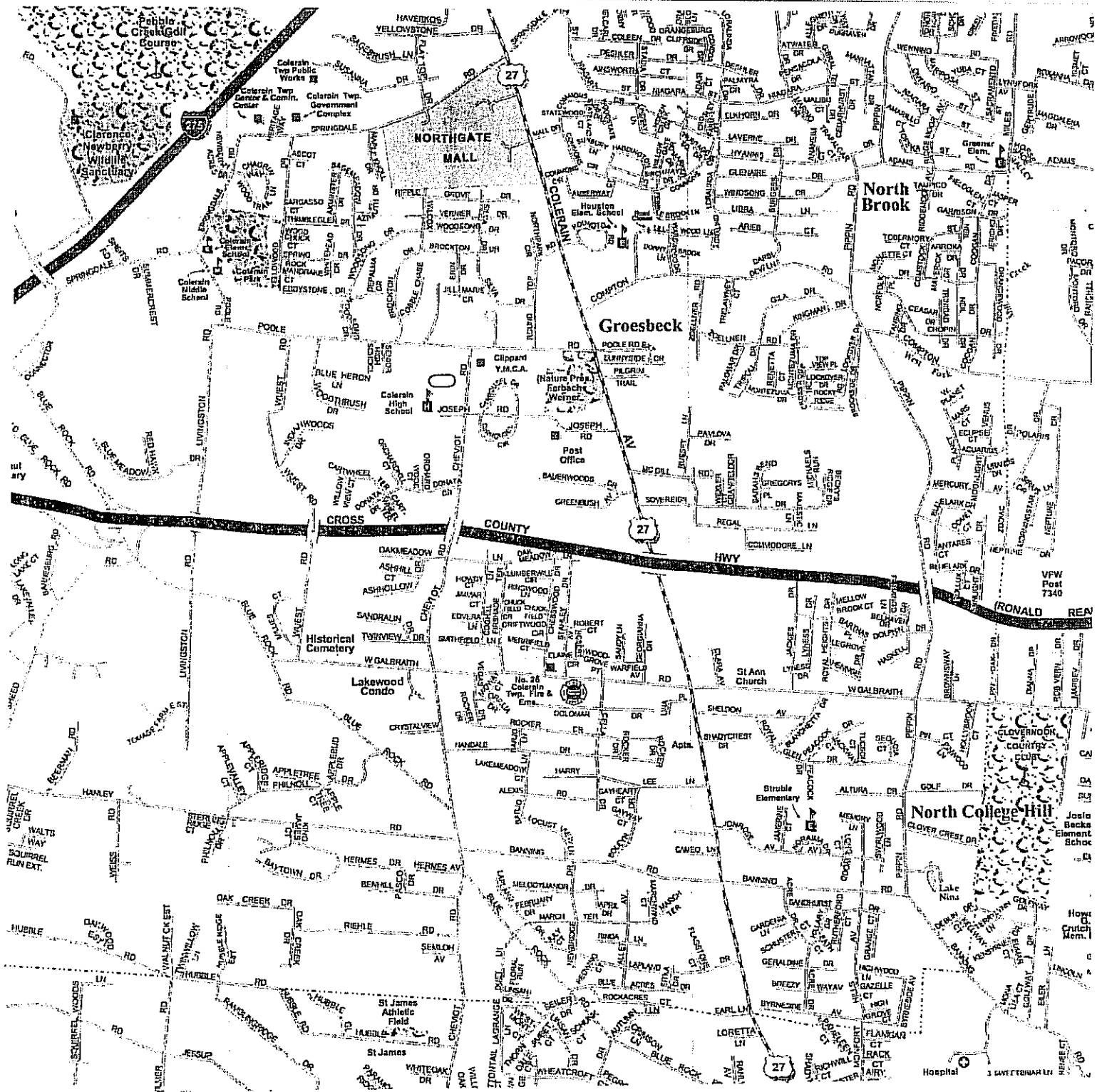
The source of the local match will be Colerain Township funds. Local matching funds will be encumbered and certified upon completion of the Project Agreement with the Ohio Public Works Commission.

## COLERAIN TOWNSHIP

Chief Executive Officer: David L. Foglesong  
David L. Foglesong, Administrator  
Colerain Township

Chief Financial Officer: Heather E. Harlow  
Heather E. Harlow, Fiscal Officer  
Colerain Township





# BELHAVEN DRIVE AND FLAMINGO LANE

# Groesbeck

# Groesbeck

27

**HWY**

**St Ann  
Church**

**Apts**

**Struble  
Elementary**

(RONAL

CLOVERNO  
COUNTRY  
CLUB



# Colerain Township

**Trustees**  
KEITH N. CORMAN  
BERNARD A. FIEDELDEY JR.  
JEFFREY F. RITTER  
**Fiscal Officer**  
HEATHER E. HARLOW  
**Administrator**  
DAVID L. FOGLESONG

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(513) 385-7502 • FAX (513) 245-6163 • www.coleraintwp.org

RESOLUTION No. 63-08

Hamilton County, Ohio

Be It Resolved by the Township Trustees of Colerain Township,  
That

WHEREAS Colerain Township has the opportunity to apply in 2008 for SCIP / LTIP funds from the Ohio Public Works Commission for Round 23 for reconstruction on various streets in Colerain Township as listed on Attachment "A", and

WHEREAS A Chief Executive Officer, a Financial Officer, and a Project Manager must be appointed to enter into a contract with the Ohio Public Works Commission; now therefore,

BE IT  
RESOLVED That the Colerain Township Board of Trustees hereby appoints Colerain Township Administrator David L. Fogleong as Chief Executive Officer; Colerain Township Fiscal Officer Heather Harlow as Financial Officer, and Colerain Township Public Works Road Superintendent Bruce E. McClain as Project Manager.

Adopted the 12th day of August, 2008

Attest: Heather E. Harlow  
Fiscal Officer

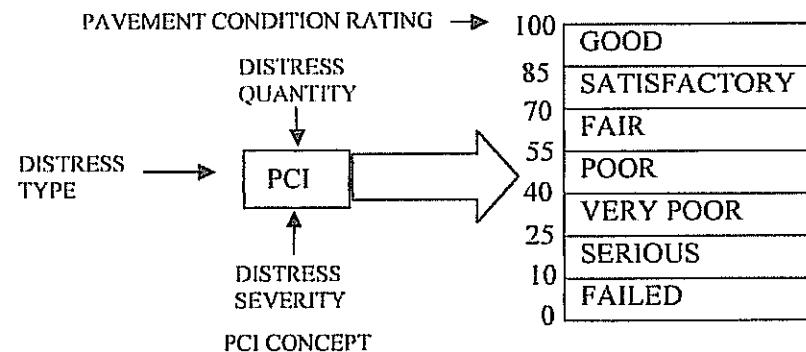
Township Trustees

**COLERAIN TOWNSHIP**  
**PAVEMENT MANAGEMENT SYSTEM**  
**MICRO PAVER**

Colerain Township uses Micro Paver, a computerized Pavement Management System. It is a decision making tool which allows the Township to develop cost effective maintenance and repair alternatives for Township roads. Hamilton County Engineers also use Micro Paver as their Pavement Management System.

The computerized system consists of a database to store the information, programs and procedures to search, retrieve and analyze the data. The data for this is taken from the field inspections by a qualified field inspector.

The U.S. Army Construction Engineering Research Laboratory (USACERL) developed the Micro Paver Pavement Management System to optimize the use of pavement repair funds. The system, which uses state-of-the-art management techniques, was developed through funding from the U.S. Army, U.S. Air Force, Federal Aviation Administration (FAA), and Federal Highway Administration (FHWA). The American Public Works Association (APWA) provides and made available the Micro Paver system to public agencies, providing educational training courses, distribution, and full technical support of the system for established fees. APWA has contributed significantly through monitoring paver field testing by many cities and providing feedback to the development team. An important factor in optimizing the use of pavement repair funds is the pavement condition, which is determined by using the Pavement Condition Index (PCI).



The PCI is an objective and repeatable rating of pavement condition based on observed distress. The PCI provides a consistent measure of a pavement's structural integrity and operational condition. The condition prediction will give a predicted PCI, which in turn shows the rate at which these pavements deteriorate. The combination of the PCI and predicted PCI generated these streets applied for on this SCIP application.

The rating methods described here were developed over many years by the U.S. Army Construction Engineers Research Lab (CERL). The methods are designed to result in a composite pavement "index" which would reflect the rating given by a very experienced and knowledgeable pavement engineer. The definitions have gone through scores of iterations of rewriting and field-testing and those presented here have been field tested by the APWA Research Foundation, during the cooperatively funded project "Optimizing Pavement Investments". The APWA study found that these methods result in consistent PCI ratings regardless of inspector, provided that the inspector is properly trained. Colerain Township has been working with Micro Paver since 1990. It has been an asset to our Pavement Management.

# SECTION IDENTIFICATION SKETCH

Surveyed By: Dodd Bandy

Installation Name	Date	Branch Name / Branch No.	Section No.	Zone	Length	Width	Area
478	4-10-2008	Belhaven	1	A	428 ft.	25 ft.	10,700 S.F.

Branch Use	Section Cat.	Pavement Rank	Surface Type	Slab	Last Const. Date
<u>Roadway</u> Runway Helipad	A B C D	P S <u>T</u> X N	AC ACC APC		
Parking Taxiway Motorpool	E F G H	OR	ST ABR <u>PCC</u>	Width 12 Length 15	mm dd yy
Other Apron Storage	I J K N	A B C D E	GR BR X	Total No. 59 Slabs	3/10/59

From Haskell Dr. To End Total No. of Sample Units 1

On Sketch: Note any Drainage Structures (type, location) and Secondary Structures, such as Manholes, Water Valves, Etc.

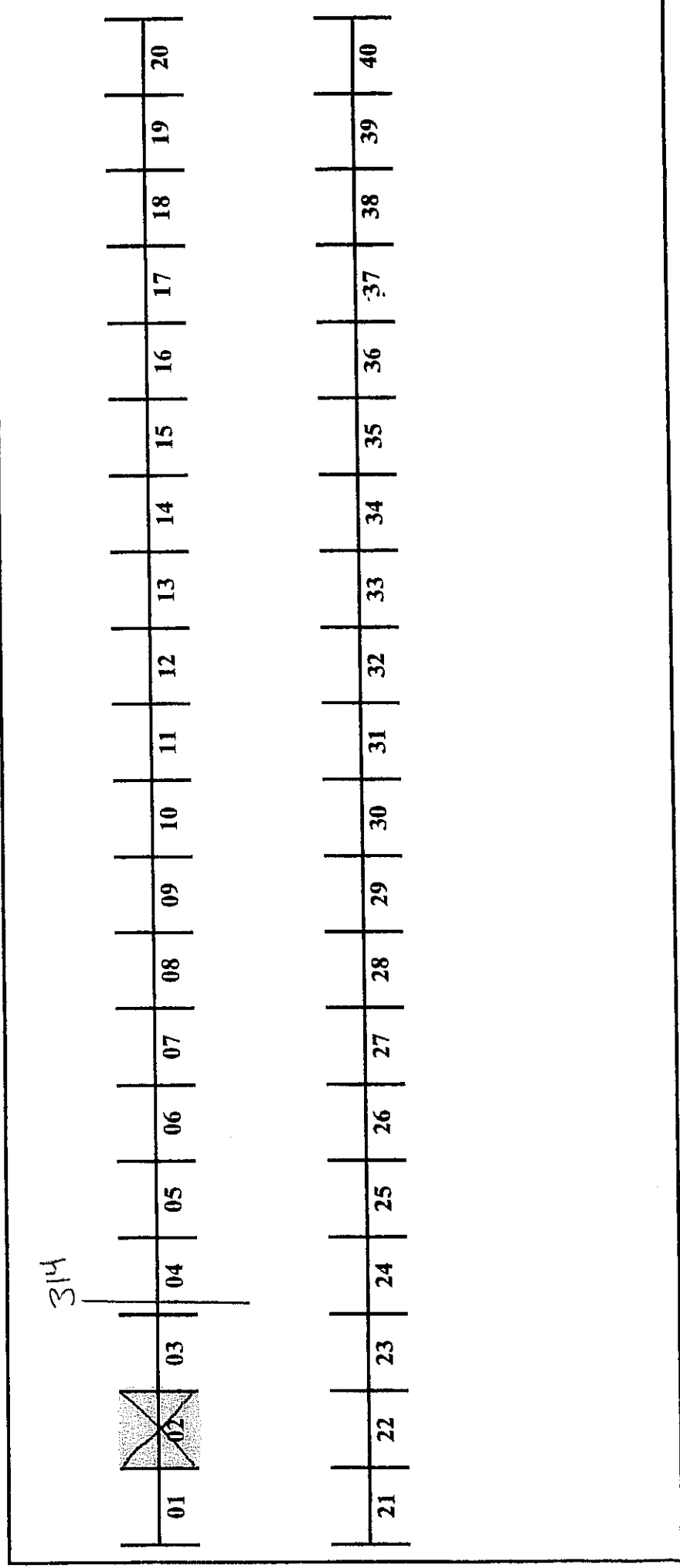


FIGURE 11 .

[illegible]

**Distresses Are Counted On A Slab-By-Slab Basis Except Address 26, Which Is Rated For The Entire Sample Unit.**

# Re-Inspection Report

COLTRN

Report Generated Date: 8/20/2008

Site Name:

Network: 1 Name: COLERAIN TOWNSHIP PUBLIC WORKS DEPARTMENT ROAD DIVISION

Branch: 0478 Name: BELHAVEN DR Use: ROADWAY Area: 10,700.00SqFt

Section: 1 of 1 From: HASKEL DR To: END Last Const.: 3/10/1959

Surface: PCC Family: DEFAULT Zone: A Category: Rank: T

Area: 10,700.00SqFt Length: 428.00Ft Width: 25.00Ft

Slabs: 59 Slab Width: 12.00Ft Slab Length: 15.00Ft Joint Length: 1,152.00Ft

Shoulder: Street Type: Grade: 0.00 Lanes: 0

Section Comments: Used Date houses were built for

Last Insp. Date: 4/10/2008 Total Samples: 1 Surveyed: 1

Conditions:

Inspection Comments:

Sample Number: 2 Type: R Area: 14.00Slabs PCI = 0

Sample Comments:

26 JOINT SEAL DAMAGE	H	14.00 Slabs	Comments:
23 DIVIDED SLAB	H	8.00 Slabs	Comments:
24 DURABILITY CRACKING	H	14.00 Slabs	Comments:
25 FAULTING	H	10.00 Slabs	Comments:
25 FAULTING	M	2.00 Slabs	Comments:
86 SCALING/CRAZING	H	2.00 Slabs	Comments:
39 JOINT SPALLING	H	4.00 Slabs	Comments:

# SECTION IDENTIFICATION SKETCH

Surveyed By: Joe D. Bandy

Installation Name	Date	Branch Name / Branch No.	Section No.	Zone	Length	Width	Area
479	4-10-2008	Flamingo Lane	1	A	3/4 ft.	25 ft.	7850 S.F.

Branch Use	Section Cat.	Pavement Rank	Surface Type	Slab	Last Const. Date
<u>Roadway</u>	A B C D	P S <u>T</u> X N	AC ACC APC		
Parking	E F G H	OR	ST ABR <u>PCC</u>	Width <u>12</u> Length <u>15</u>	
Other Apron Storage	I J K N	A B C D E	GR BR X	Total No. _____ Slabs	mm dd yy 3 10 15

From Belhaven Dr. To End Total No. of Sample Units 1

On Sketch: Note any Drainage Structures (type, location) and Secondary Structures, such as Manholes, Water Valves, Etc.

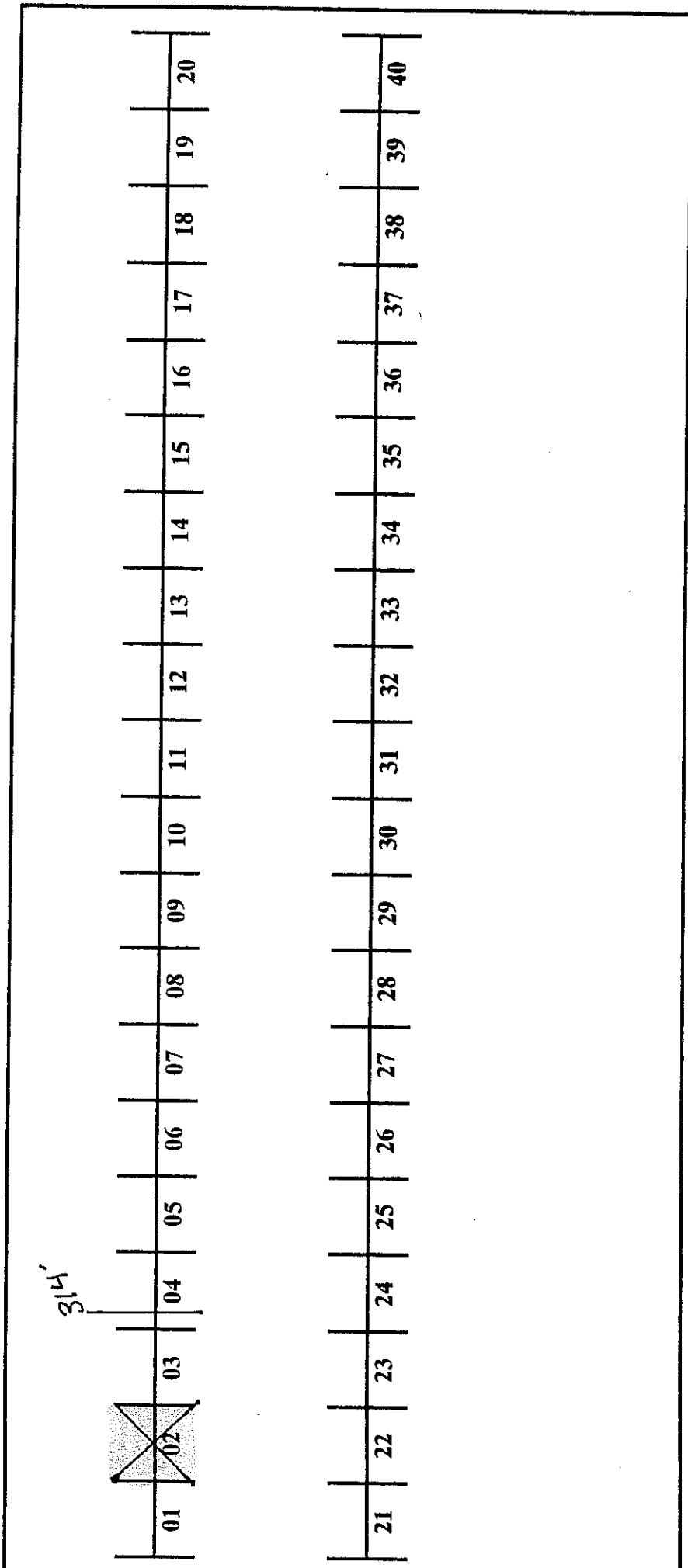




FIGURE 11 -

[illegible]

*Distresses Are Counted On A Slab-By-Slab Basis Except Distress 26, Which Is Rated For The Entire Sample Unit.*

# Re-Inspection Report

COLTRN

Report Generated Date: 8/20/2008

Site Name:

Network: 1 Name: COLERAIN TOWNSHIP PUBLIC WORKS DEPARTMENT ROAD DIVISION

Branch: 0479 Name: FLAMINGO LN Use: ROADWAY Area: 7,850.00SqFt

Section: 1 of 1 From: BELHAVEN DR To: END Last Const.: 3/10/1957

Surface: PCC Family: DEFAULT Zone: A Category: Rank: T

Area: 7,850.00SqFt Length: 314.00Ft Width: 25.00Ft

Slabs: 44 Slab Width: 12.00Ft Slab Length: 15.00Ft Joint Length: 838.50Ft

Shoulder: Street Type: Grade: 0.00 Lanes: 0

Section Comments:

Last Insp. Date: 4/10/2008 Total Samples: 1 Surveyed: 1

Conditions:

Inspection Comments:

Sample Number: 2 Type: R Area: 14.00Slabs PCI = 18

Sample Comments:

26 JOINT SEAL DAMAGE	H	14.00 Slabs	Comments:
25 FAULTING	H	3.00 Slabs	Comments:
28 LINEAR CRACKING	M	1.00 Slabs	Comments:
29 LARGE PATCH/UTILITY CUT	L	2.00 Slabs	Comments:
38 CORNER SPALLING	M	3.00 Slabs	Comments:
39 JOINT SPALLING	L	1.00 Slabs	Comments:
39 JOINT SPALLING	M	2.00 Slabs	Comments:
39 JOINT SPALLING	H	4.00 Slabs	Comments:
24 DURABILITY CRACKING	H	6.00 Slabs	Comments:
23 DIVIDED SLAB	H	1.00 Slabs	Comments:
24 DURABILITY CRACKING	M	2.00 Slabs	Comments:



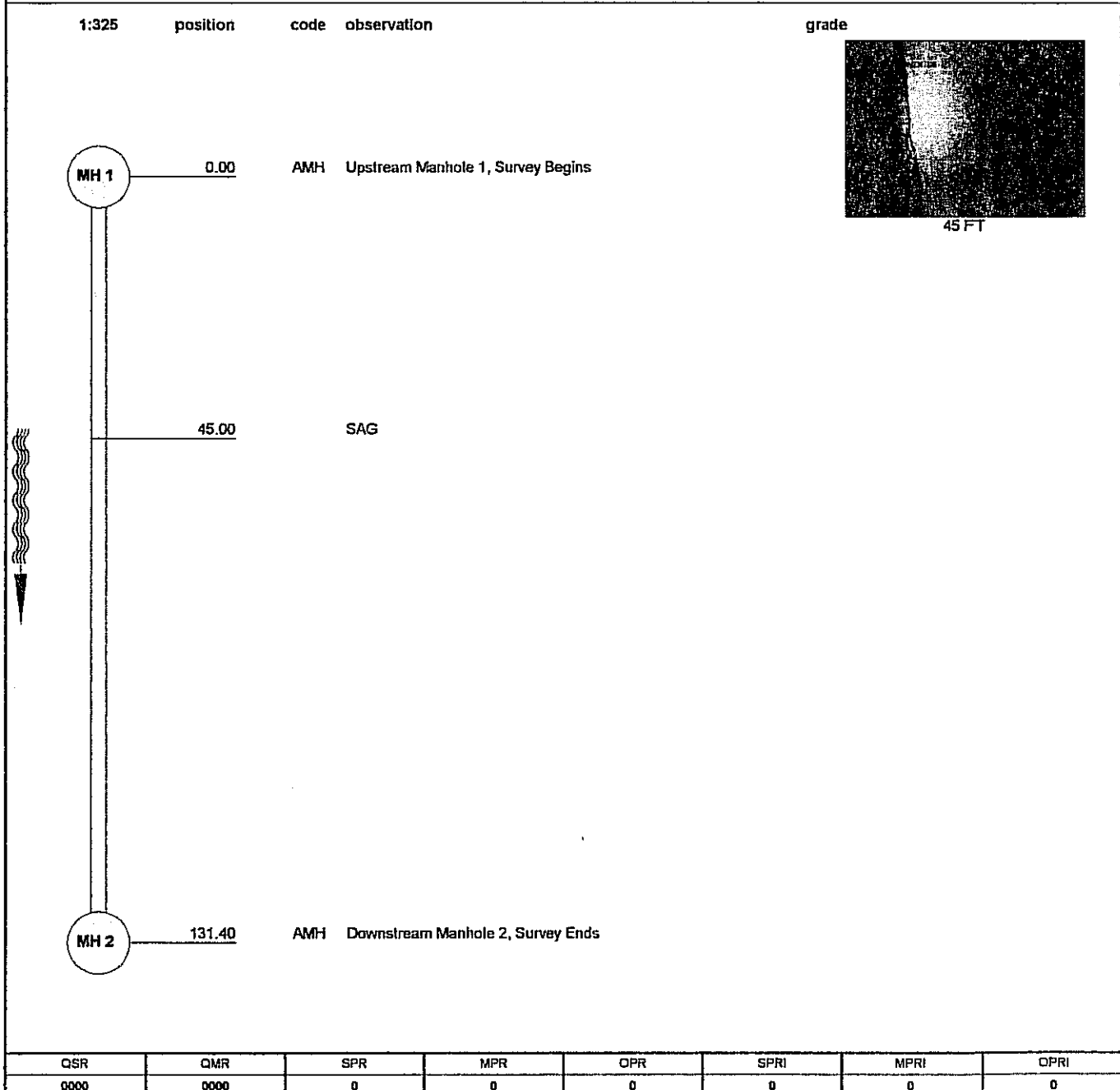
## Inspection report

Date: 02/13/2008	P.O.#:	Weather: 4 Snow	Surveyed By: DUKE	section number: 3	PSR:
Total Pipe Length:	Survey Customer:	System Owner: TELE-VAC	Clean Date:	Pre-Cleaned: N No Pre-Cleaning	Map Grid #:

Street: 2574 BELHAVEN DR	Flow Control:	Start MH: MH 1
City: COLERAIN TWSP.	Year Renewed	End MH: MH 2
Location Code: D Easement/Right of Way	Tape/Media #: 001	Total length: 131.4 ft

Purpose: A Maintenance Related	Dia/Height: C Circular 12"/12"
Use: SW Stormwater	Material: RCP Reinforced Concrete Pipe Pipe length:
Drain. Area:	Lining:
	Category:

Comment:  
Location details:





TELEVAC eNVIronMENTAL  
7611 eASY sT.  
mASON,oh 45040  
Tel: (513) 398-4521, Fax: (513) 398-5628

## Inspection photos

City: <b>COLERAJN TWSP.</b>	Street: <b>2574 BELHAVEN DR</b>	Date: <b>02/13/2008</b>	section number: <b>3</b>	PSR:
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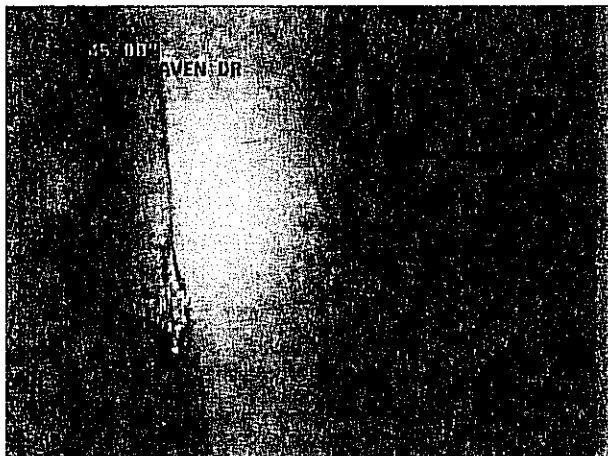


Photo: 20a  
45FT, SAG



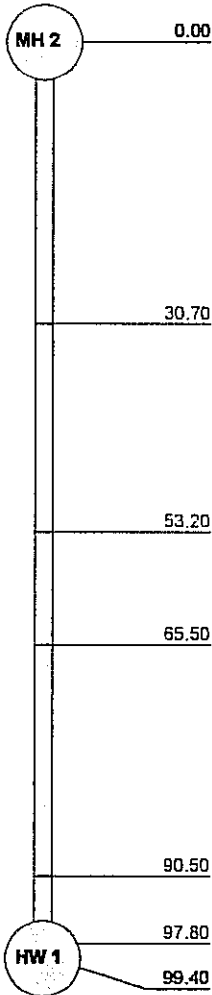
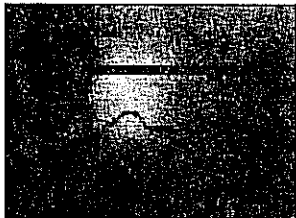
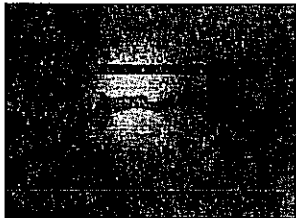
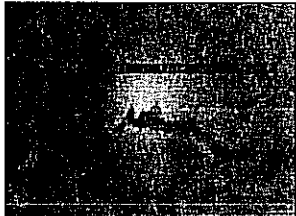
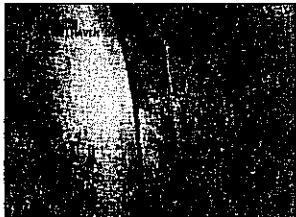


## Inspection report

Date: 02/13/2008	P.O.#:	Weather: 4 Snow	Surveyed By: DUKE	section number: 4	PSR:
Total Pipe Length:	Survey Customer:	System Owner: TELE-VAC	Clean Date:	Pre-Cleaned: N No Pre-Cleaning	Map Grid #:

Street: 2574 BELHAVEN DR	Flow Control:	Start MH: MH 2
City: COLERAIN TWSP.	Year Renewed	End MH: HW 1
Location Code: D Easement/Right of Way	Tape/Media #: 001	Total length: 99.4 ft

Purpose: A Maintenance Related	Dia/Height: C Circular 12"/12"
Use: SW Stormwater	Material: RCP ReInforced Concrete Pipe Pipe length:
Drain. Area:	Lining:
	Category:

Comment:  
Location details:

1:250	position	code	observation	grade			
	0.00	AMH	Upstream Manhole 2, Survey Begins	 30.7 FT			
	30.70	CL	Crack Longitudinal, at 12 o'clock, within 8 inch: NO	 53.2 FT			
	53.20	CL	Crack Longitudinal, at 12 o'clock, within 8 inch: NO	 65.5 FT			
	65.50	CL	Crack Longitudinal, at 12 o'clock, within 8 inch: NO	 90.5 FT			
	90.50	JSL	Joint Separated Large, Remark: SOIL VISSIBLE	 90.5 FT			
	97.80	FC	Fracture Circumferential, from 12 to 12 o'clock, within 8 inch: NO, Remark: SOIL VISSBLE	 90.5 FT			
	99.40	AMH	Downstream HW 1 Survey Ends				
QSR	QMR	SPR	MPR	OPR	SPRI	MPRI	OPRI
2500	0000	10	0	10	2	0	2



## Inspection photos

City:	Street:	Date:	section number:	PSR:
COLERAIN TWSP.	2574 BELHAVEN DR	02/13/2008	4	

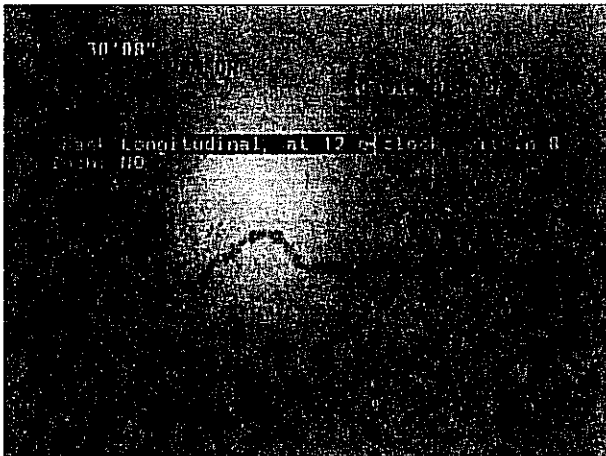


Photo: 23a  
30.7FT, Crack Longitudinal, at 12 o'clock, within 8 inch: NO

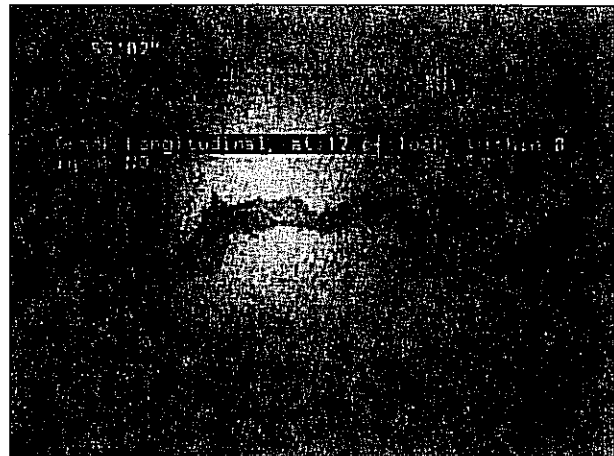


Photo: 24a  
53.2FT, Crack Longitudinal, at 12 o'clock, within 8 inch: NO

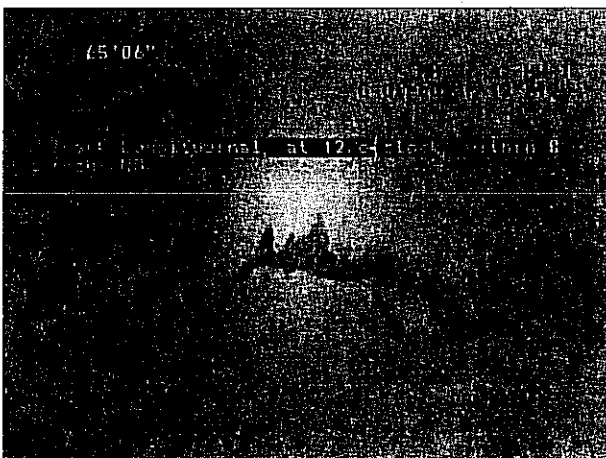


Photo: 25a  
65.5FT, Crack Longitudinal, at 12 o'clock, within 8 inch: NO



Photo: 26a  
90.5FT, Joint Separated Large, Remark: SOIL VISSIBLE



## Inspection photos

City: <b>COLERAIN TWSP.</b>	Street: <b>2574 BELHAVEN DR</b>	Date: <b>02/13/2008</b>	section number: <b>4</b>	PSR:
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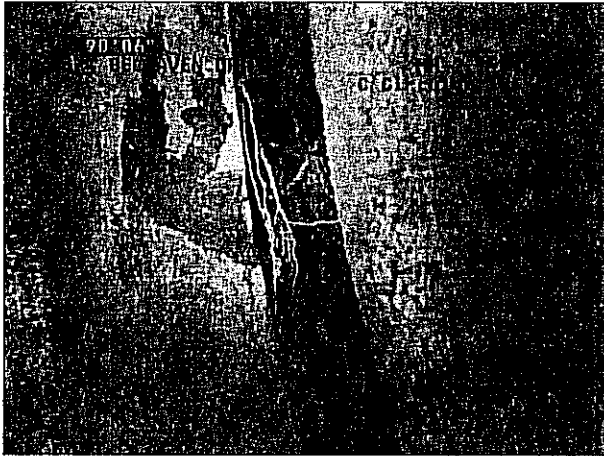


Photo: 26b  
90.5FT, Joint Separated Large, Remark: SOIL VISSIBLE



Photo: 27a  
97.8FT, Fracture Circumferential, from 12 to 12 o'clock, within 8 inch: NO, Remark: SOIL VISSBLE



Photo: 27b  
97.8FT, Fracture Circumferential, from 12 to 12 o'clock, within 8 inch: NO, Remark: SOIL VISSBLE



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7611 eASY sT.  
mASON,oh 45040  
Tel: (513) 398-4521, Fax: (513) 398-5028

## Inspection report

Date: 02/13/2008	P.O.#:	Weather: 4 Snow	Surveyed By: DUKE	section number: 5	PSR:
Total Pipe Length:	Survey Customer:	System Owner: TELE-VAC	Clean Date:	Pre-Cleaned: N No Pre-Cleaning	Map Grid #:

Street: 2574 BELHAVEN DR	Flow Control:	Start MH: MH 1
City: COLERAIN TWSP.	Year Renewed	End MH: C/B 1
Location Code: D Easement/Right of Way	Tape/Media #: 001	Total length: 19.2 ft

Purpose: A Maintenance Related	Dia/Height: C Circular 12"/12"
Use: SW Stormwater	Material: RCP Reinforced Concrete Pipe Pipe length:
Drain. Area:	Lining:
	Category:

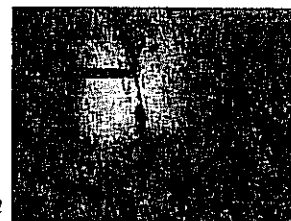
Comment:  
Location details:

1:50 position code observation

grade

MH 1 0.00 AMH Downstream Manhole 1, Survey Begins  
0.00 JOL Joint Offset Large

S 2  
0 FT



C/B 1 19.20 AMH Upstream CB 1 Survey Ends

QSR	QMR	SPR	MPR	OPR	SPRI	MPRI	OPRI
2100	0000	2	0	2	2	0	2





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## Inspection photos

City: COLERAIN TWSP.	Street: 2574 BELHAVEN DR	Date: 02/13/2008	section number: 5	PSR:
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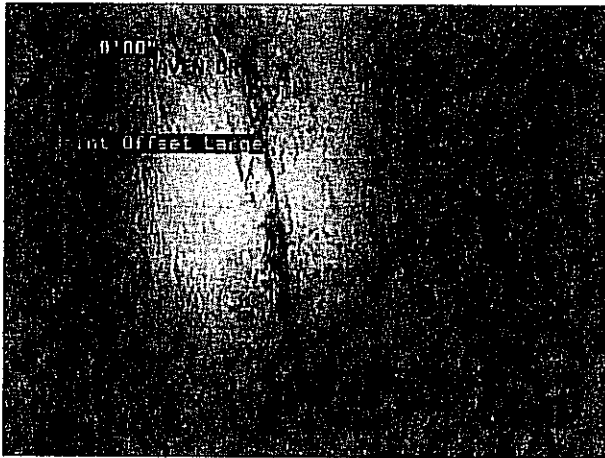


Photo: 31a  
OFT, Joint Offset Large



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mASON,oh 45040  
Tel: (513) 398-4521, Fax: (513) 398-5628

## Inspection report

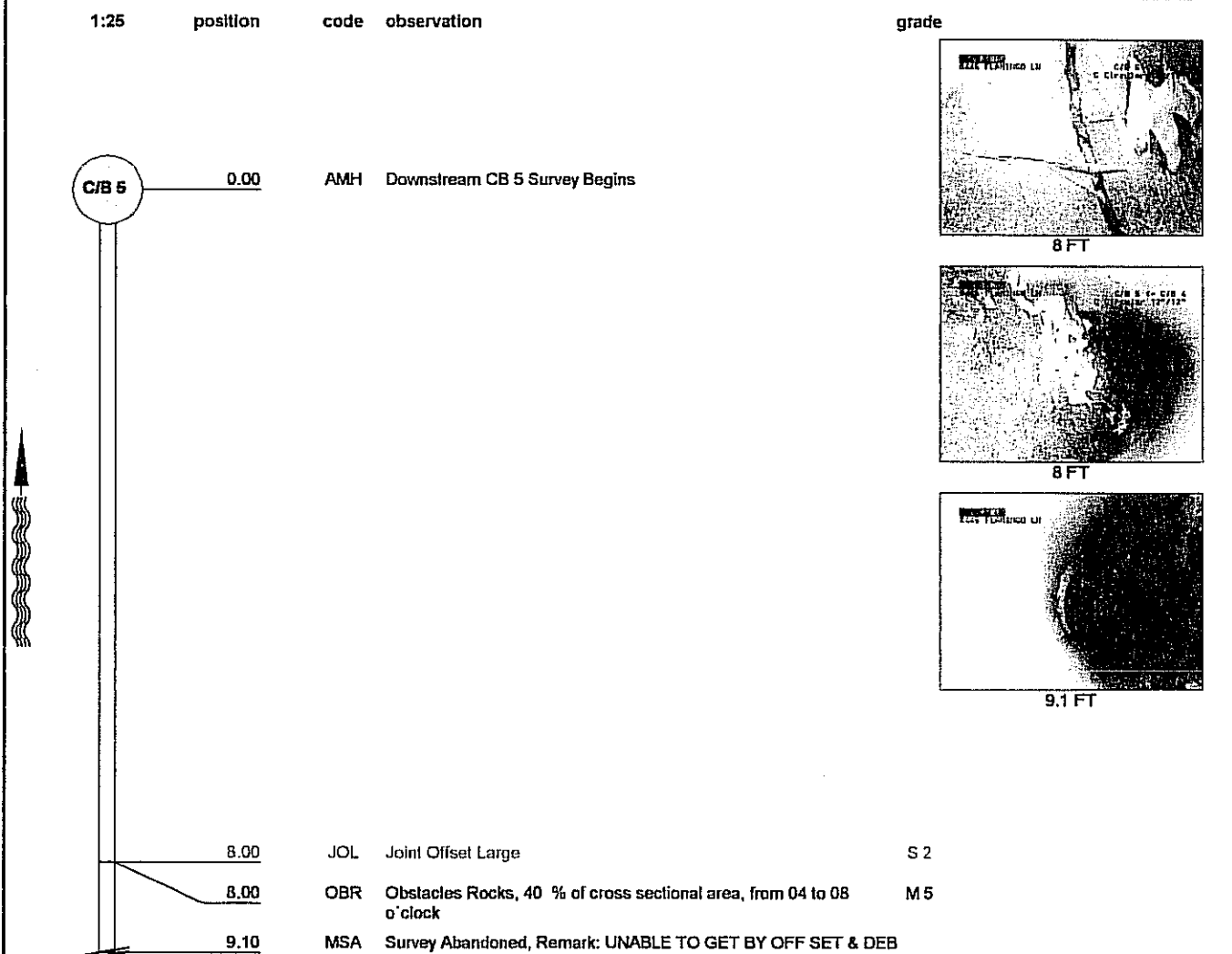
Date: 02/13/2008	P.O.#:	Weather: 4 Snow	Surveyed By: DUKE	section number: 1	PSR:
Total Pipe Length:	Survey Customer:	System Owner: TELE-VAC	Clean Date:	Pre-Cleaned: N No Pre-Cleaning	Map Grid #:

Street: 8445 FLAMINGO LN	Flow Control:	Start MH: C/B 5
City: COLERAIN TWSP.	Year Renewed	End MH: C/B 4
Location Code: D Easement/Right of Way	Tape/Media #: 001	Total length: 9.1 ft

Purpose: A Maintenance Related	Dia/Height: C Circular 12"/12"
Use: SW Stormwater	Material: RCP Reinforced Concrete Pipe Pipe length:
Drain. Area:	Lining:
	Category:

Comment:

Location details:



QSR	QMR	SPR	MPR	OPR	SPRI	MPRI	OPRI
2100	6100	2	5	7	2	5	3.5



## Inspection photos

City: <b>COLERAIN TWSP.</b>	Street: <b>8445 FLAMINGO LN</b>	Date: <b>02/13/2008</b>	section number: <b>1</b>	PSR:
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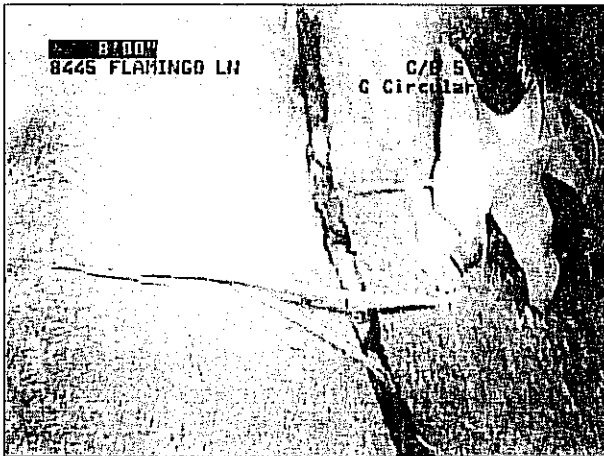


Photo: 3a  
8FT, Joint Offset Large

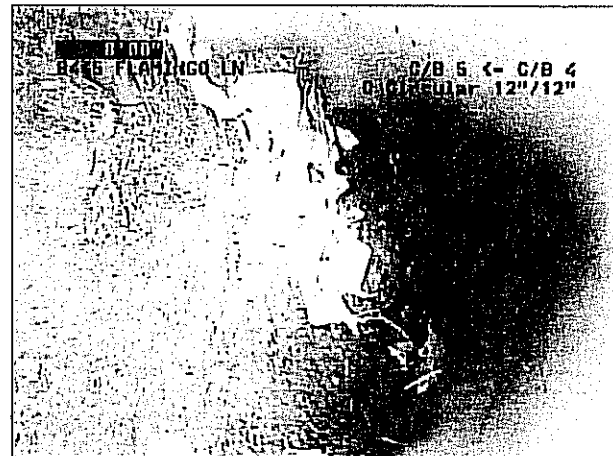


Photo: 4a  
8FT, Obstacles Rocks, 40 % of cross sectional area, from 04 to 08 o'clock

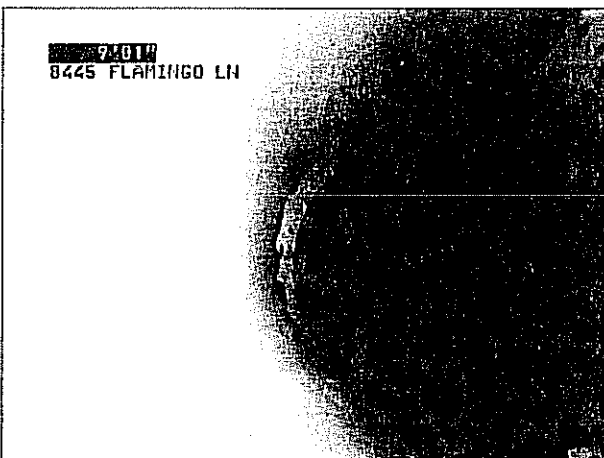


Photo: 5a  
9.1FT, Survey Abandoned, Remark: UNABLE TO GET BY OFF SET & DEB



## Inspection report

Date: 02/13/2008	P.O.#:	Weather: 4 Snow	Surveyed By: DUKE	section number: 2	PSR:
Total Pipe Length:	Survey Customer:	System Owner: TELE-VAC	Clean Date:	Pre-Cleaned: N No Pre-Cleaning	Map Grid #:

Street: 8445 FLAMINGO LN	Flow Control:	Start MH: C/B 5
City: COLERAIN TWSP.	Year Renewed	End MH: HW 2
Location Code: D Easement/Right of Way	Tape/Media #: 001	Total length: 152.2 ft

Purpose: A Maintenance Related	Dia/Height: C Circular 12"/12"
Use: SW Stormwater	Material: RCP Reinforced Concrete Pipe
Drain. Area:	Pipe length:
	Lining:
	Category:

Comment:

Location details:

1:375	position	code	observation	grade				
<div>Diagram of a vertical pipe section with manholes C/B 5 at the top and HW 2 at the bottom. Inspection points are marked along the pipe with depth measurements and codes. A north arrow is on the left.</div>	0.00	AMH	Upstream CB 5 Survey Begins	<div>16.4 FT</div>				
	16.40	JOL	Joint Offset Large	S 2				
	16.40	HSV	Hole Soil Visible, from 09 to 12 o'clock, within 8 inch: YES / SAG BEGINS	S 5				
	20.70	HSV	Hole Soil Visible, from 04 to 08 o'clock, within 8 inch: YES	S 5				
	20.70		BAD SAG HOLDING DEBRIS					
	24.20	JOL	Joint Offset Large, Remark: W, HOLE SOIL VISSBLE	S 2				
	24.20		END OF SAG					
	28.80	FM	Fracture Multiple, from 12 to 12 o'clock, within 8 inch: YES	S 4				
	44.90	JOL	Joint Offset Large, Remark: W, SOIL VISSIBLE	S 2				
	78.10	S1	CL Crack Longitudinal, at 12 o'clock, within 8 inch: NO, start	S 2				
	86.50	F1	CL Crack Longitudinal, at 12 o'clock, within 8 inch: NO, end	S 2				
	127.70	IW	Infiltration Weeper, from 12 to 12 o'clock, within 8 inch: YES	M 2				
	152.20	AMH	Downstream HW 2 Survey Ends	<div>24.2 FT</div>				
	QSR	QMR	SPR	MPR	OPR	SPRI	MPRI	OPRI
	5241	2100	27	2	29	3	2	2.9



## Inspection photos

City: <b>COLERAIN TWSP.</b>	Street: <b>8445 FLAMINGO LN</b>	Date: <b>02/13/2008</b>	section number: <b>2</b>	PSR:
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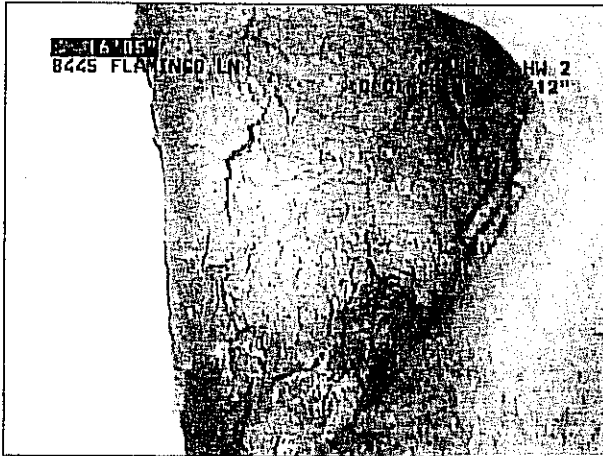


Photo: 6a  
16.4FT, Joint Offset Large

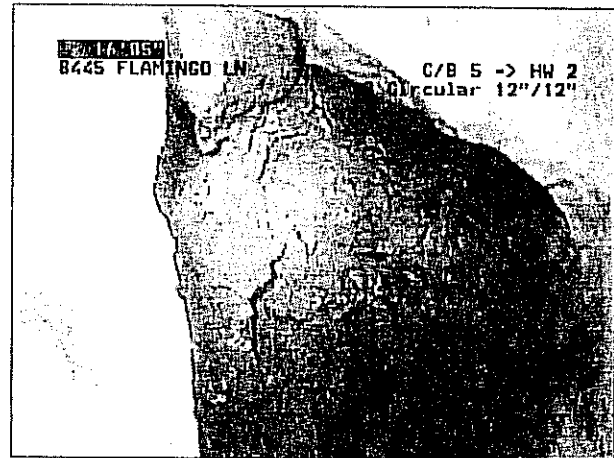


Photo: 7a  
16.4FT, Hole Soil Visible, from 09 to 12 o'clock, within 8 inch:  
YES / SAG BEGINS

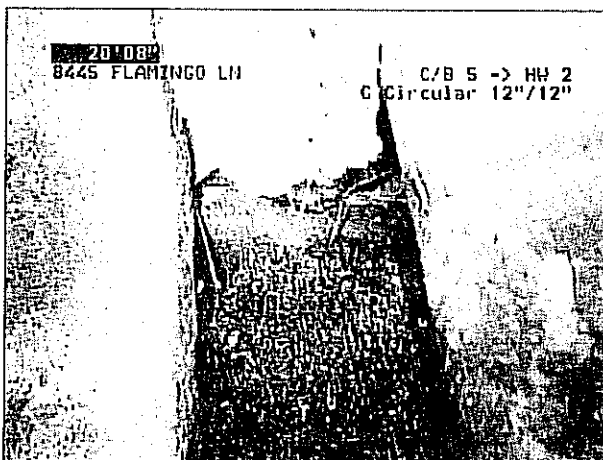


Photo: 8a  
20.7FT, Hole Soil Visible, from 04 to 08 o'clock, within 8 inch:  
YES

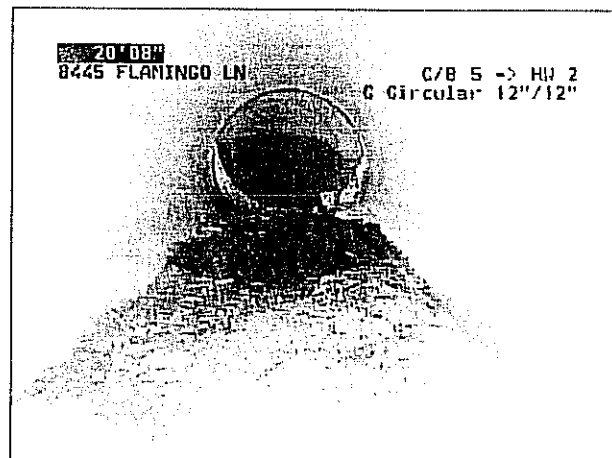


Photo: 9a  
20.7FT, BAD SAG HOLDING DEBRIS



## Inspection photos

City: COLERAIN TWSP.	Street: 8445 FLAMINGO LN	Date: 02/13/2008	section number: 2	PSR:
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Photo: 10a  
24.2FT, Joint Offset Large, Remark: W, HOLE SOIL VISSBLE

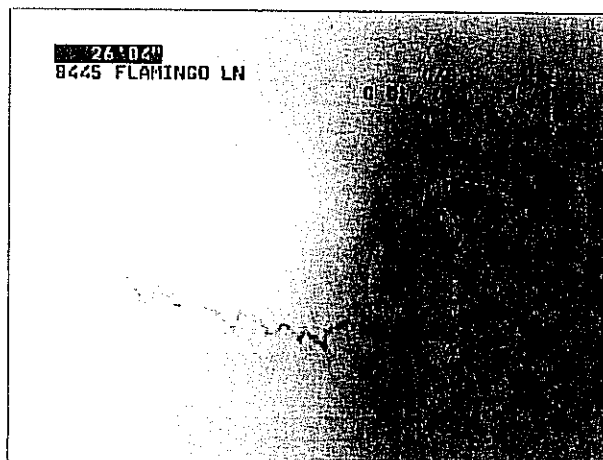


Photo: 12a  
26.3FT, Fracture Longitudinal, at 12 o'clock, within 8 inch: YES,  
Remark: & 6 O/C POSITION

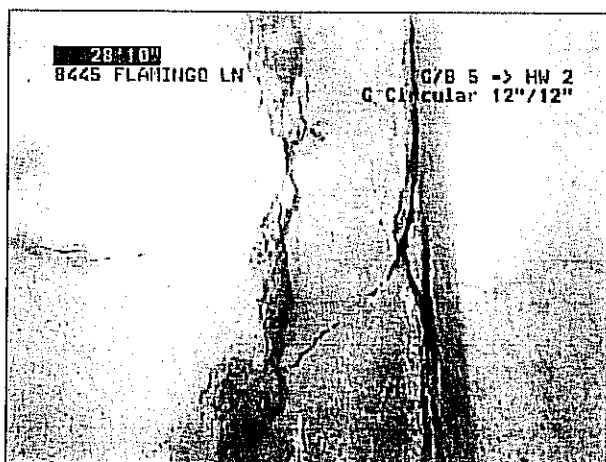


Photo: 13a  
28.8FT, Fracture Multiple, from 12 to 12 o'clock, within 8 inch:  
YES



Photo: 14a  
44.9FT, Joint Offset Large, Remark: W, SOIL VISSIBLE



## Inspection photos

City:	Street:	Date:	section number:	PSR:
COLERAIN TWSP.	8445 FLAMINGO LN	02/13/2008	2	

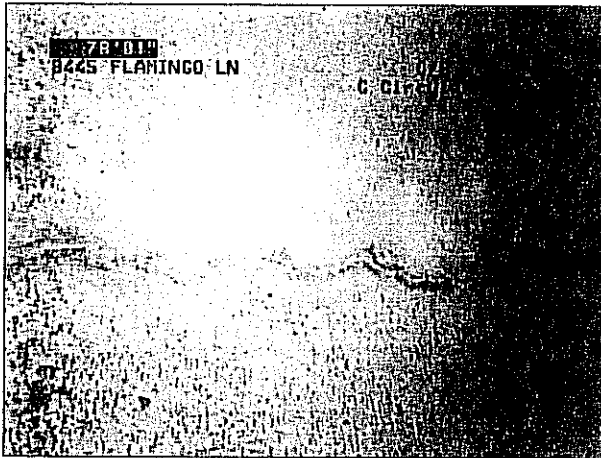


Photo: 15a  
78.1FT, Crack Longitudinal, at 12 o'clock, within 8 inch: NO, start

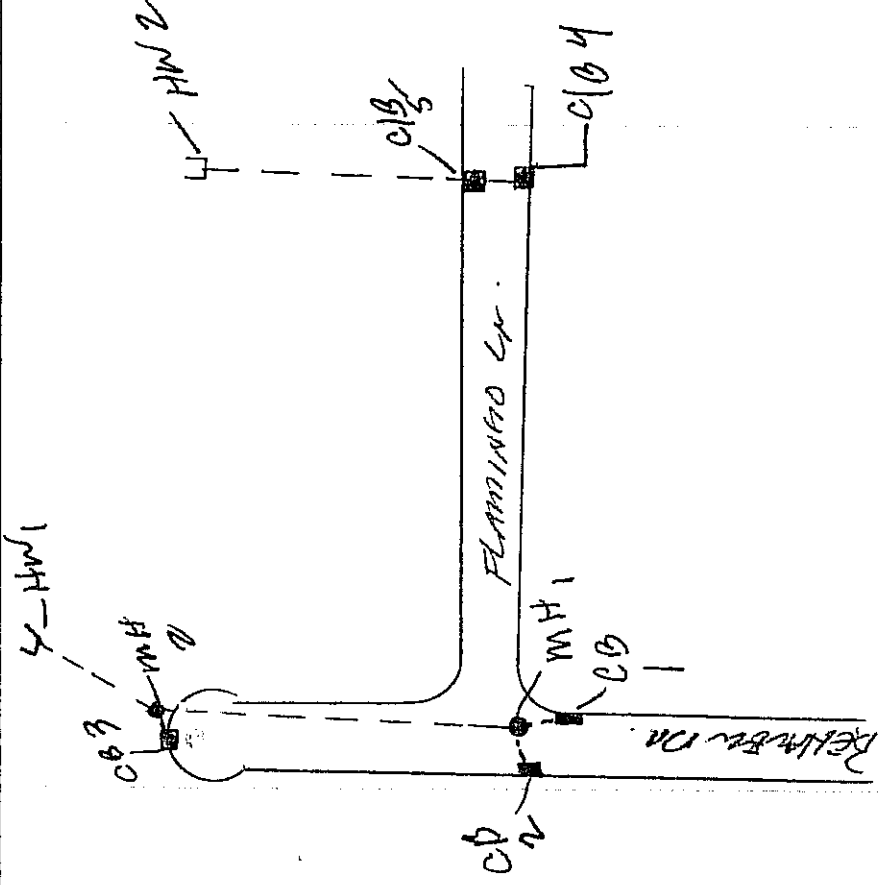


Photo: 16a  
86.5FT, Crack Longitudinal, at 12 o'clock, within 8 inch: NO, end



Photo: 17a  
127.7FT, Infiltration Weeper, from 12 to 12 o'clock, within 8 inch: YES

	Date	Place	Road	Start MH	Stop MH	Material	Shape	tot. Length
1	02/13/2008	COLERAIN TWSP.	8445 FLAMINGO LN	C/B 5	C/B 4	RCP Reinforced Concrete Pip	C Circular 12"/12"	9.1
2	02/13/2008	COLERAIN TWSP.	8445 FLAMINGO LN	C/B 5	HW 2	RCP Reinforced Concrete Pip	C Circular 12"/12"	152.2
3	02/13/2008	COLERAIN TWSP.	2574 BELHAVEN DR	MH 1	MH 2	RCP Reinforced Concrete Pip	C Circular 12"/12"	131.4
4	02/13/2008	COLERAIN TWSP.	2574 BELHAVEN DR	MH 2	HW 1	RCP Reinforced Concrete Pip	C Circular 12"/12"	99.4
5	02/13/2008	COLERAIN TWSP.	2574 BELHAVEN DR	MH 1	C/B 1	RCP Reinforced Concrete Pip	C Circular 12"/12"	19.2
6	02/13/2008	COLERAIN TWSP.	2574 BELHAVEN DR	MH 1	C/B 2	RCP Reinforced Concrete Pip	C Circular 12"/12"	16.2





March 10, 1965

Mr. Dwight L. Ricknell  
The Cincinnati Enquirer  
617 Vine St.,  
Cincinnati, Ohio 45202

Dear "Dick":

This is the reply to the recent letter addressed to you and forwarded to this office, written by Mr. Wesley Mues in behalf of seventeen residents on Belhaven Drive and Flamingo Lane in Grassland Subdivision, Colerain Township.

Subject subdivision was approved for construction in 1952. This was prior to the Ohio State Subdivision Law which became effective in October 1953. This law provides, that (any lot transferred shall front on an accepted public road, or a road guaranteed by surety posted with the County to become a public road). The developer, by having authorization prior to the Subdivision Law recorded a plan of subject subdivision without provision for the dedication and acceptance of the roads. A title examination of the lots on the two roads should clearly have indicated this situation. The developer later submitted a dedication plat and withdrew same rather than complete the improvements in accordance with County standards.

There exists no bond to guarantee the completion, because the subdivision law does not apply.

Under these circumstances the Township Trustees are not obligated to accept these roads and complete them at public expense, nor are they required to provide snow removal, road name and traffic signs or other services on private roads.

Other items of question: Discontinuation of mail service being discontinued. The local post office can answer this question. - As to whether or not a lending institution will lend money on housed lots that front on undedicated roads- loans are based on the value of the property and house, and contingent on a clear deed to the property.

Mr. Dwight L. Dickrell  
Cincinnati, Ohio  
2.

A solution to the problem would require the street area to be dedicated by the developer and through some arrangement provide for the correction and/or completion of the necessary improvements to County standards and the satisfaction of the Township Trustees, thereby clearing the way for acceptance of the streets.

Very sincerely yours,

HENRY W. ROLLINS  
COUNTY ENGINEER

ED  
WRH:vl



# County of Hamilton

DONALD C. SCHRAMM, P.E.-P.S. COUNTY ENGINEER

700 COUNTY ADMINISTRATION BUILDING  
138 EAST COURT STREET  
CINCINNATI, OHIO 45202  
GENERAL INFORMATION (513) 632-8523

December 21, 1987

Colerain Township  
4725 Springdale Road  
Cincinnati, OH 45239

Attention: Mr. Dennis B. Chapman  
Director of Public Works

RE: Grassland Subdivision  
Belhaven Dr. & Flamingo Lane

Dear Mr. Chapman:

Pursuant to your request by letter dated 11/25/87, this office has made a field review of Belhaven Drive and Flamingo Lane in Grassland Subdivision to ascertain condition of surface facilities. The following information represents our findings:

## MANHOLES:

Belhaven Drive - Due to apparent pavement (and subgrade) settlement, three (3) sanitary manholes should be adjusted to grade at approximately stations 3+47, 3+73 and 4+45.

## PAVEMENT:

Belhaven Drive - Due to apparent settlement of a storm sewer trench, a fifteen foot (15') section of pavement at approximately station 3+73, left of centerline should be replaced.

Belhaven Drive - Due to a combination of poor construction practice and settlement, a section of pavement approximately ten feet by twenty feet (10' x 20') at the sanitary manhole, station 4+45 should be replaced.

Flamingo Lane - Due to settlement a fifteen foot (15')+ full width (right and left of centerline) section of pavement station 2+73+ should be replaced.

DOWNSPOUTS:

Flamingo Lane - Downspouts serving houses #8436 and #8444 are not connected through the curb cuts and should be corrected.

Please note that the "zero" station for Belhaven Drive is the centerline of Haskell Drive; and for Flamingo Lane the centerline of Belhaven Drive. In addition, it was noted that at the north end of Flamingo Lane, a six inch (6") diameter tree is interfering with access to the fire hydrant and should be removed.

The following deficiencies were noted which should not affect any maintenance responsibility of your township if streets are dedicated.

Settled drive aprons at the following address: 8421, 8429, 8437, and 8445 Flamingo Lane.

Uneven displaced sidewalks at the following addresses: 2561 (5'), 2577 (5') and on right side of cul-de-sac Belhaven Drive (20'), 8421 (10'), 8426 (10'), 8429 (10'), 8437 (20'), 8436-8444 (15') and 8445 (20') Flamingo Lane. We wish to point out that the sidewalk status is not too unlike what now exists on other dedicated streets in the subdivision.

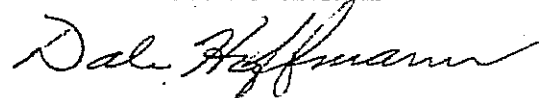
Should these two streets become dedicated, they should be considered as early candidates for joint sealing and overlay.

Mr. James Nimz of our Construction Department has informed me that they have no records for Glasslands Subdivision.

We trust the foregoing provides you with ample information regarding our assessment of surface conditions.

Very truly yours,

DONALD C. SCHRAMM, P.E.-P.S.  
HAMILTON COUNTY ENGINEER



Dale Hoffmann  
Subdivision Technician

DCS/DRH/gl

cc: Jim Nimz  
Office file

# MEMO

COPY

March 31, 1994

**TO:** Patricia Clancy, Keith Miller, Joseph Wolterman, Kathy Mohr, David Foglesong

**FROM:** Dennis

I attended a meeting March 28, 1994 in which approximately 14 residents from Grasslands Subdivision, Bellhaven Drive and Flamingo Lane, attended. These two streets were built back around 1957-58 and are concrete streets with concrete curb and gutter. Most of the sidewalk has been installed and most of the lots have been built on. The two streets are owned by people who live out of town and have not been adequately maintained. The residents that attended the meeting are interested in having the streets made public so that they will be able to have the services provided as a township street.

I had explained the procedure and the complications that they have by having someone other than the residents own the land that is the street and right of way. That the Township expects the streets to be in a maintained condition to Hamilton County Standards for Subdivision Streets before accepting any streets. The streets have held up well for their age, but the problems that we have with most concrete streets is also prevalent here, there are some blocks that have dropped, others have voids underneath, and it would be necessary for full and partial depth repairs to be made.

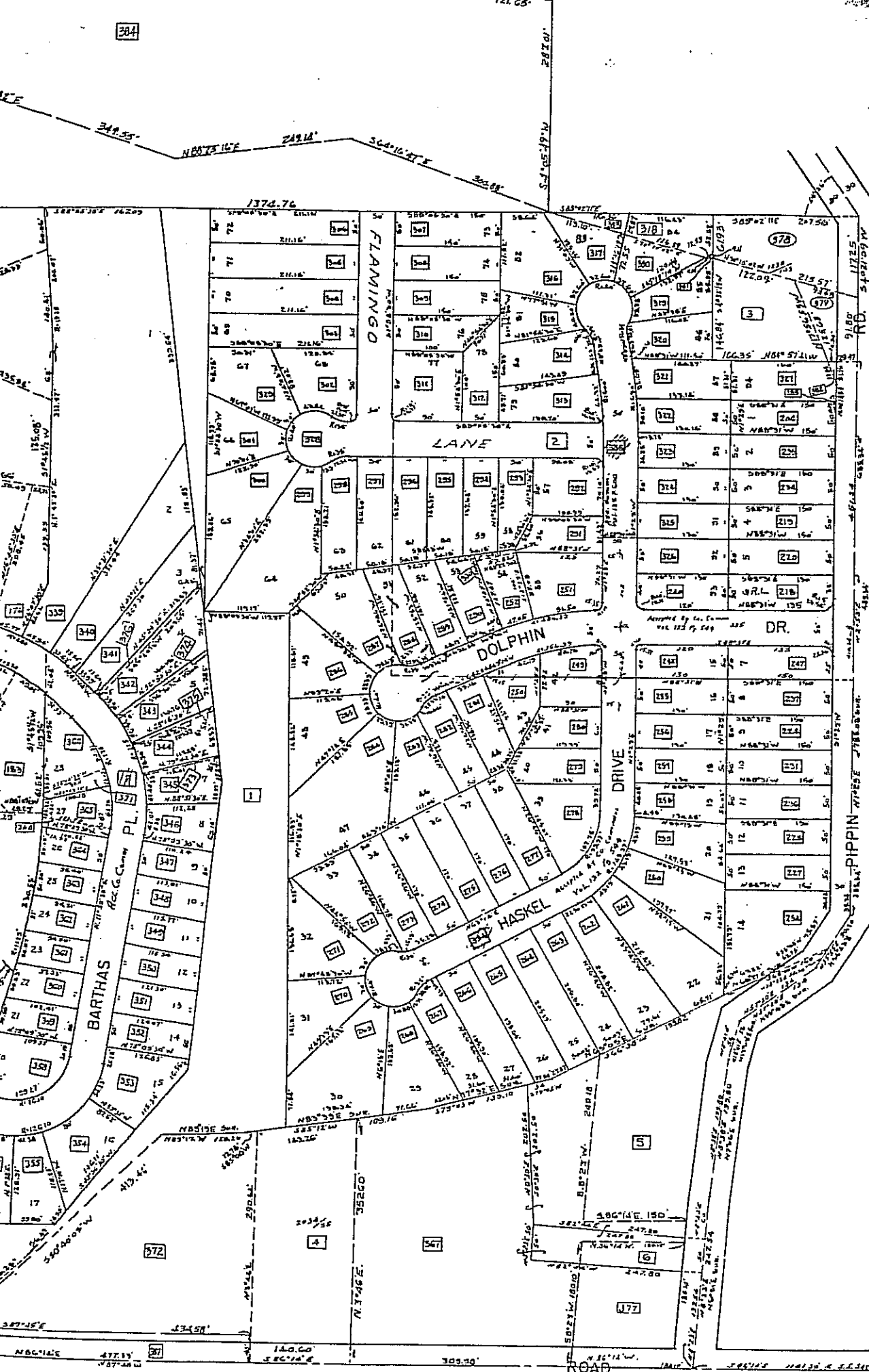
The residents are planning to come to the next Township meeting with a couple of questions in the area of 1) will the Township take the streets the way they are and make them public, 2) will the Township make the repairs and access the property owner, and 3) are there any funds the Township could make the repairs with. I have answered these questions to the residents that were present this way; 1) the streets are private and that the Township could not expend public funds on private property, 2) that the assessment process I am only familiar with on public right-of-way but I did not feel that it would be able to be used on private property, the Board would have to consider if it is permissible.

These two streets were built prior to current subdivision regulations and were probably a major part of the reason the regulations are what they are today and that the subdivider is not able to put streets in and sell lots without the right-of-way being dedicated for public use and posting a performance bond in a sufficient amount to complete the subdivision as for the approved plans.

This is not the first time that it has been brought up that these streets would be considered for public acceptance and dedication. I feel certain that these people, even though some have bought houses recently, moved in without the real knowledge that they were moving on anything but a public street because of the physical appearance. This departments' desire would be, because of this nature, to help in what ever legal means we can and the Board would direct. Again I do not feel that this is a normal private street and that its residents could decide they would like it to be a public street. There are no panhandles coming out to Haskel Drive, which is the closest public street, each of their property comes out to the proposed right-of-way and this proposed right-of-way is also a parcel of land that is owned by one owner. The attachment is a copy of the print which shows at least Dolphin, Haskel, Flamingo and Bellhaven. We have highlighted Flamingo and Bellhaven to show that these are the streets that we are speaking of.

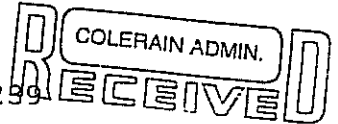
If you have any further questions or would like to go over and take a look at these streets with me, please let me know.

c: Jim Reuter



SEE 510-63

8413 Flamingo Lane  
Cincinnati, Ohio 45299  
March 31, 1994  
522-5143  
Certified mail  
Return receipt requested



APR 4 1994

Colerain Township Trustees  
4200 Springdale Rd.  
Cincinnati, Ohio 45231

Attention: Patty Clancy  
Keith Miller  
Joe Wolterman

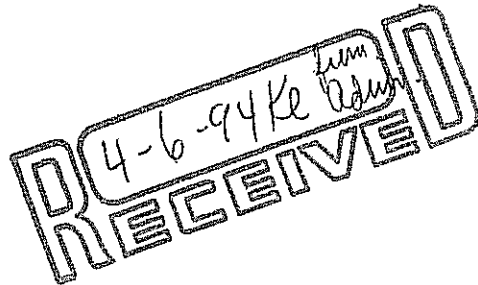
We who live on Belhaven Drive and Flamingo Lane, in Colerain Township, have a problem in that these two streets have not been dedicated/accepted by the Township. They are privately owned and have not been properly maintained, so that today we play "dodge" as we drive on them in order to avoid the pot holes and general deterioration.

I have been instructed by the other residents to ask if the Township can make the necessary repairs for dedication of these streets. We, individually or collectively, are not financially able to do this.

We will appreciate your giving this matter your sincere consideration, and will ask that if you have any suggestions you will give them to us before the streets become impassable.

Yours truly,

A handwritten signature in cursive script that reads "Juanita K. Pence".  
JUANITA K. PENCE



C: Board  
adm  
Plw - for response



# Memo

To: Dennis

From: Debi

Date: January 21, 2003

Re: **FOLLOW-UP SUMMARY** - Flamingo Lane & Belhaven Drive

---

Dennis,

The Grassland Subdivision Block "C" was approved for construction in 1952. Prior to the "Ohio State Subdivision Law" set in 1953. The developer recorded a plan for dedication without the provision for the dedication and acceptance. I don't have a name for the developer, I have copies of two other plats from apparently two more attempts at dedication. The first in 1959 submitted by Ayers & Graf, The second in 1987 submitted by B. L. Payne & Associates.

*anyone has these plat?*

On April 6, 1960 The Hamilton County Regional Planning Commission requested a "Record Plat" for Bond. On April 7, 1960 The Hamilton County Engineer's Office requested the sanitary sewers and drainage structures be checked for bond purposes. On April 15, 1960 an estimate for bond was submitted in the amount of \$6,500.00 by someone with the initials of D.P. from the Engineer's office. The developer submitted a plan and withdrew it rather than complete the improvements to county standards.

On March 10, 1965, the Engineers office sent a response letter (draft copy attached) to Mr. Bicknell of the Cincinnati Enquirer explaining these circumstances, (a copy of Mr. Bicknell's letter was not available). The letter stated that there was no maintenance bond and that the developer would need to make the necessary corrections to County standards and the satisfaction of the Township Trustees

On January 20, 1966 the Hamilton County Sanitary Engineering Department composed a punch list (attached) to be completed, It was sent to Ken George by Don Valentine, of the Sanitary Engineering Department.

On April 27, 1987 Colerain Township received a letter from Lois Beddard, the owner of Bellhaven and Flamingo, requesting the necessary procedures to have these streets accepted by the Township. This office forwarded this letter to the County Engineer on May 1<sup>st</sup>, 1987 requesting the status with the developer and the Counties position on this matter.

We received a letter dated May 29, 1987 stating a dedication plat needs to be prepared and signed by the owner (Ms. Beddard), and that the County Public Works Department be contacted to determine the condition of the underground structures. Included with this letter was a punch list from Dale Hoffman dated May 27, 1987.

In November 25, 1987 this office requested from the County Engineer a contact person and an assessment of the surface conditions. On December 21, 1987, additions were made to the punch list sent by Dale on May 27<sup>th</sup> and sent to the Colerain Township Public Works Department. This letter also stated that Mr. James Nimz of the county Construction Department had no records for Grassland Subdivision.

- Colerain Township and the County Prosecutor's Office show Daniel Beddard as the current owner for these parcels.
- The Auditors office shows foreclosure in 1994 due to back taxes but no current owner.
- The County Road Records Department has no further information on this issue.
- Larry Baron of the Prosecutor's Office performed a title search in 1994.
- The Prosecutor's Office cannot pursue ownership of parcels that are actually streets.

Mr. Baron recommends that the property owners hire an Attorney to perform a private title search so they can establish these streets as private or abandoned. If considered abandoned, I believe it would be approached as a vacation of property, (which would make these parcels belong to each of the abutting property owners). New deeds would then be drawn with surveys and legal descriptions. After ownership has been established, the property owners can try to obtain funding to bring these streets up to code for dedication.

John Beck had provided most of this information along with an estimate of costs necessary to bring these streets up to code.

Debi

GRASSLAND SUBDIVISION

8/16/02 JMB

777' TOTAL PAVEMENT LENGTH

FLAMINGO LANE 325 L.F.

PAVEMENT 325x\$80 : \$26,000 ASPHALT

325x\$110 : \$35,750 CONCRETE

SIDEWALK (325x2)x\$12 : \$7,800

REMOVAL WALK (325x2)x\$10 = \$6,500

REMOVAL PVMT 325x\$25 = \$8,125

SOD RESTORATION : \$4,000

\$26,425

CLERK TOWNSHIP MAN

OCT 8 2002

RECEIVED

BELHAVEN DRIVE 452.29 L.F.

PAVEMENT 452x\$80 : \$36,160 ASPHALT

452x\$110 : \$49,720 CONCRETE

SIDEWALK (452x2)x\$12 : \$10,848

REMOVAL WALK (452x2)x\$10 = \$9,040

REMOVAL PVMT 452x\$25 = \$11,300

SOD RESTORATION : \$6,000

\$37,188

ASPHALT PAVEMENT REPLACEMENT OPTION : \$125,773 \*

CONCRETE " " " : \$149,083 \*

\* DOES NOT INCLUDE ANY INFRASTRUCTURE REPLACEMENT. THOSE COSTS ARE ADDITIONAL.

## Bruce McClain

---

**From:** Beck, John [John.Beck@hamilton-co.org]  
**Sent:** Wednesday, November 29, 2006 1:08 PM  
**To:** Sellmeyer, Judy  
**Cc:** Bruce McClain  
**Subject:** Flamingo Lane & Belhaven Drive - Colerain Township

Judy,

I know it's been a while, but have we heard anything from the Prosecutor's Office on this? This particular subdivision (D.B.N.A.) goes back to the late 1950's and the owner/developer is deceased. The street R/W needs to be turned over to the BOCC in order for Colerain to either do a total reconstruct or apply for SCIP funds next round and hope it gets funded. This is Grassland Subdivision.

Thank you,  
John

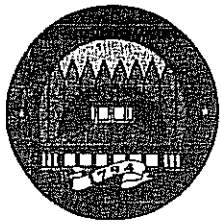
-----Original Message-----

**From:** Bruce McClain [mailto:bmcclain@coleraintwp.org]  
**Sent:** Wednesday, November 29, 2006 12:02 PM  
**To:** john.beck@hamilton-co.org  
**Subject:** Flamingo Lane & Belhaven Drive

John,  
Greetings.  
Can you check with your contact on the progress the Prosecutors Office is making on the above subject?  
Thank you!  
Bruce McClain

This e-mail message, including any attachments, is for the sole use of the intended recipient(s) and may contain private, confidential and/or privileged information. Any unauthorized review, use, disclosure or distribution is prohibited. If you are not the intended recipient, employee or agent responsible for delivering this message, please contact the sender by reply e-mail and destroy all copies of the original e-mail message.

*1/29/07 John will follow up*



# Colerain Township

**Trustees**  
KEITH N. CORMAN  
BERNARD A. FIEDELDEY JR.  
JEFF RITTER  
**Fiscal Officer**  
HEATHER E. HARLOW  
**Administrator**  
DAVID L. FOGLESONG

## PUBLIC WORKS DEPARTMENT, ROAD DIVISION

**Bruce McClain, Road Superintendent**

4160 Springdale Road • Colerain Township, Ohio 45251-1834  
(513) 385-7502 • FAX (513) 245-6163 • [www.coleraintwp.org](http://www.coleraintwp.org)

March 23, 2006

William W. Brayshaw, P.E.-P.S.  
Hamilton County Engineer  
10480 Burlington Road  
Cincinnati, Ohio 45231

Re: Flamingo Lane and Belhaven Drive; Street Dedication and Acceptance

Dear Mr. Brayshaw,

Greetings.

Colerain Township respectfully requests the County Engineer's opinion on the above-mentioned matter.

- What is the status of ownership?
- What would be the process and procedure to get these two streets dedicated and accepted?

Enclosed is a copy of the most recent correspondence we have on file.

Mr. Joe Serbling, the resident mentioned within the correspondence resides at:

8437 Flamingo Lane  
Colerain Township, Ohio 4529  
Cell # 513-470-5707

Your assistance is always appreciated.

Sincerely,

*Bruce E. McClain*

Bruce E. McClain  
Road Superintendent

*Mailed  
3/24  
PB*

**COLERAIN TOWNSHIP  
PUBLIC WORKS  
ROAD DIVISION**

# MEMO

copy

---

To: Board of Trustees, David  
From: Dennis *DC*  
Date: March 17, 2003  
Re: Joe Sterbling; Flamingo Lane and Belhaven Drive; Street Dedication and Acceptance

---

I had a conference call with Joe Sterbling and John Beck of the County Engineer's Subdivision Office on Monday March 17<sup>th</sup>. John Beck stated that his office is going to do a title search in house and that the County Engineer's Office would move forward with getting these streets made public.

He stated that no one in the County Engineer's Office had dealt with this type of situation before but would do their best to correct the problem. The streets were owned by an individual as real property and the County foreclosed on the parcel.

We had told Mr. Sterbling that the Prosecutors Office stated they cannot pursue ownership of parcels that are actually streets. They had stated that the property owners would need to hire an attorney to perform their legal work for them. John had stated that he would contact the Prosecutors Office to see how much they could get done by the Prosecutor.

Mr. Sterbling's said that he had the address of the owner of the parcel and was going to get that for John Beck.

If you have any further questions, please feel free to contact me.

*DC*  
3/18/03

Debbie Weller

---

**From:** Beck, John [John.Beck@hamilton-co.org]  
**Sent:** Friday, April 28, 2006 2:53 PM  
**To:** Beck, Eric; Beck, John; Bob Bass (E-mail); Cottrill, Joe; Dick Cline (E-mail); Greg Long; John Knuf (E-mail); Rob Molloy (E-mail); Shefcik, Bill; Williams, Bryan  
**Cc:** Bruce McClain; Debbie Weller; Sellmeyer, Judy  
**Subject:** Belhaven & Flamingo - Colerain Township



belhavenflamingo1.  
JPG

**To all:**

**Attached is a location map of 2 streets highlighted in Colerain Township. The streets are dedicated (Record Plat dated May 1959) but have never been accepted as public streets. I believe the original developer is deceased and there is no claim to ownership of the right-of-way. Our office will be looking into the right-of-way issues to see if they can be cleared up and the right-of-way put in the name of the Board of County Commissioners. If that occurs, Colerain could then possibly apply for a total reconstruct SCIP Project. The streets are in extremely poor or failed condition and I think could rate very well if an application were submitted. Utilities in the street right-of-way (water, sanitary, storm, etc.) have not been inspected, so I suspect there will be some reconstruction to those as well.**

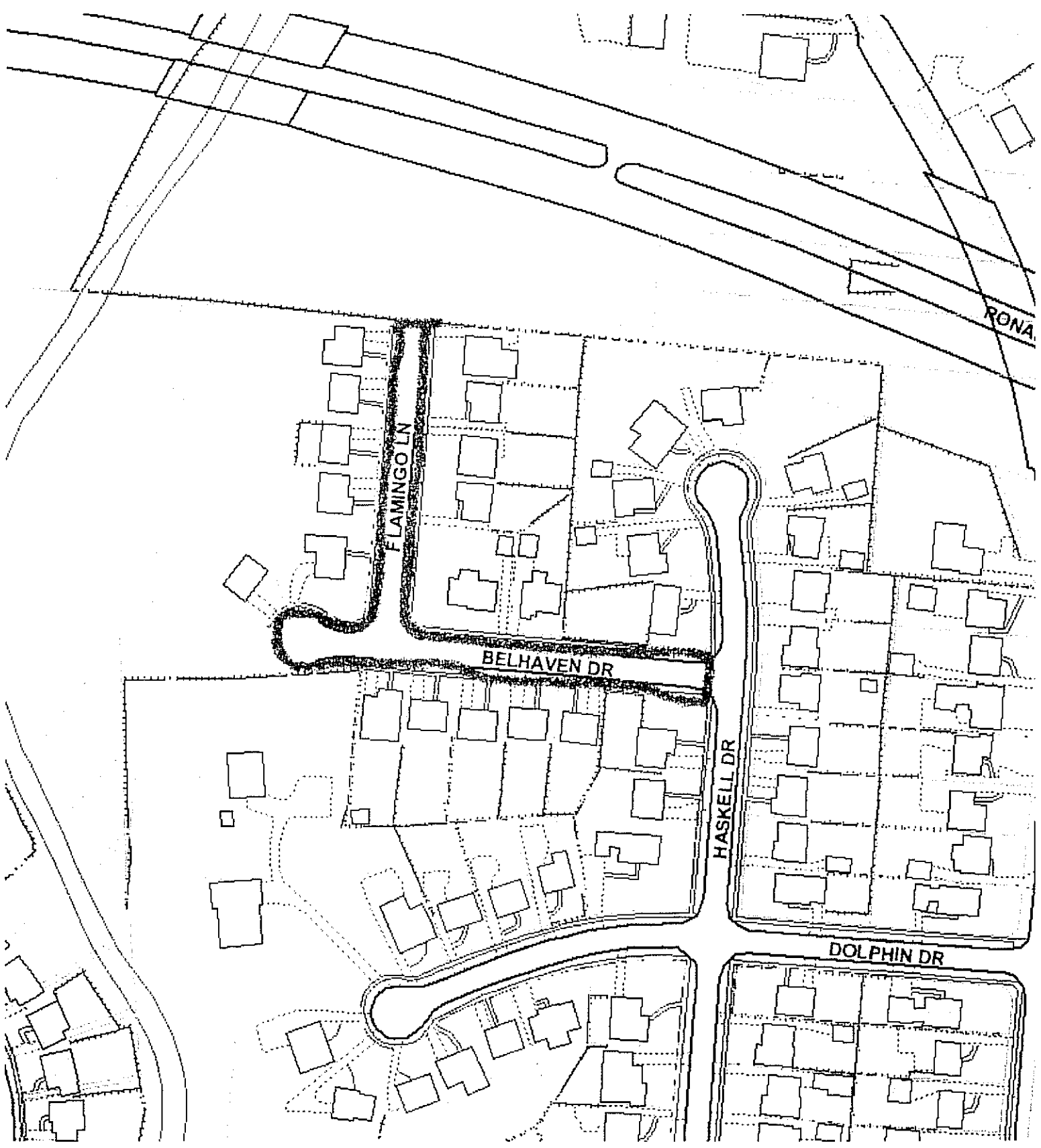
**If you have some extra time to visit the site and forward me your opinions, I would really appreciate the comments.**

**Thanks,**

**John Beck  
Hamilton County Engineer's Office  
946-4267**

**<<belhavenflamingo1.JPG>>**

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FLAMINGO LN

BELHAVEN DR

HASKELL DR

DOLPHIN DR

RONA



478 BELHAVEN DRIVE . COLERAIN TWP. 50 ' R/W

SEC. 2 T. 2 E .R. 1 L. .08 MI.

LOCATION FROM THE INTERSECTION WITH THE WEST RIGHT-OF-WAY LINE OF HASKELL DRIVE, WESTERLY A DISTANCE OF 409.29 FEET TO THE CENTER OF A TEARDROP TURNAROUND, AS SHOWN ON THE HEREIN-NAMED SUBDIVISION PLAT.

ACCEPTED BY B.O.C.C. 12/19/2007

SURVEY BOOK 52 PAGE(S) 142 CO. ENG. OFFICE

SUBDIVISION GRASSLAND SUBDIVISION BLOCK "C" AMENDED

479 FLAMINGO LANE COLERAIN TWP. 50 ' R/W

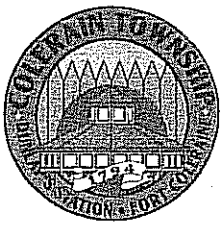
SEC. 2 T. 2 E .R. 1 L. .062 MI.

LOCATION FROM THE INTERSECTION WITH BELHAVEN DRIVE, A NORTHERLY DISTANCE OF 325.00 FEET TO THE NORTH LINE OF SAID SUBDIVISION.

ACCEPTED BY B.O.C.C. 12/19/2007

SURVEY BOOK 52 PAGE(S) 142 CO. ENG. OFFICE

SUBDIVISION GRASSLAND SUBDIVISION BLOCK "C" AMENDED.



# Colerain Township

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**Bruce McClain, Road Superintendent • Tim Lange, Road Supervisor**  
4160 Springdale Road • Colerain Township, Ohio 45251-1834  
(513) 385-7502 • FAX (513) 245-6163 • [www.coleraintwp.org](http://www.coleraintwp.org)

January 9, 2008

## SPECIAL NOTICE

### **"Belhaven Drive and Flamingo Lane become Public Streets and Township Maintained"**

Dear property owner and or resident,

When Grassland Subdivision Block "C" consisting of Belhaven Drive and Flamingo Lane were built, the streets were dedicated and built meeting the Hamilton County Engineer's public subdivision design and specifications. However, there is no record where the "Developer / Owner" of the subdivision pursued Hamilton County to accept the subdivision thus making the streets in a dedicated yet un-accepted status (which is private).

After many years of research, investigation and being unsuccessful in locating, much less contacting the developer and or owner of the Grassland Subdivision Block "C", the Hamilton County Prosecutor's Office deemed the property as abandoned on December 12, 2007. In response to that decision, The Hamilton County Commissioners accepted the above mentioned subdivision on December 19, 2007 and The Colerain Township Trustees accented to the acceptance on January 8, 2008.

Colerain Township will start maintaining these streets effective immediately. This includes snow and ice removal, street and storm sewer repairs within the right-of-way and public easements. The overall condition of the streets are poor and Colerain Township will be applying for special grants in order to obtain the needed funding to reconstruct these streets. Until the funds are obtained (and this can take years), Colerain Township will maintain the streets with repairs.

For the sake of safety and complying with the Ohio Manual of Uniform Traffic Control devices and Colerain Township subdivision standards:

- A stop sign will be placed on Flamingo Lane at Belhaven Drive.
- The streets will be posted with a 25 MPH speed limit.
- The fire hydrant sides of the streets will be posted "No Parking Anytime Fire Lane Zone" including the cul-de-sac of Belhaven Drive and the dead end of Flamingo Lane (vicinity map attached).
- Trees located between the sidewalk and street with limbs overhanging the street that could damage vehicles will be trimmed. If you wish to have the tree removed or if you wish to trim the trees limbs yourself, please contact us by January 18, 2008.

I am obliged to meet with anyone on sight or talk on the phone should there be questions, comments or concerns and can be reached at the letter head stated address, phone number or at [bmcclain@coleraintwp.org](mailto:bmcclain@coleraintwp.org).

Sincerely & at your service,

*Bruce M. McClain*

Bruce E. McClain

# COLERAIN TOWNSHIP

## PUBLIC WORKS DEPT.

Date 3/25/04 Time 9:00

A MESSAGE WAS RECEIVED

For RD

From Kenny - WMSD

8413 Starving

☐ By Phone ☐ Please Call Back

☐ In Person Number - Bus. \_\_\_\_\_

☐ Wants to See You Home \_\_\_\_\_

☐ Will Phone Again ☐ Rush

MESSAGE \_\_\_\_\_

Manholes are above

grade

Told him this street is

private, but there's for

calving

Message taken by RD

# COLERAIN TOWNSHIP

## PUBLIC WORKS DEPT

Date 4-7-2008 Time 3:30 PM

A MESSAGE WAS RECEIVED

For Pothole Repairs

From Joe

Business # \_\_\_\_\_

Home / Cell # \_\_\_\_\_

☐ By Phone ☐ Please Call Back

☒ In Person ☐ Will Call Back

☐ Wants to See You ☐ RUSH

Message \_\_\_\_\_

8486 Flamingo CURB AT DRIVEWAY

2541 BELLEVUE + 3-4 OTHER ON STREET

complete  
4/14/08

Message taken by: Joe

11-19-93

# COLERAIN TOWNSHIP

Belhaven is a Private Street  
Not Maintained by Township

## PUBLIC WORKS DEPT.

Date 11-18-93 Time 3:50 PM

A MESSAGE WAS RECEIVED

For PC

From Jimmy Hammer

2581 Belhaven Dr

☒ By Phone ☒ Please Call Back

☐ In Person 9-4 → Number - Bus. 793-8808

☐ Wants to See You Home \_\_\_\_\_

☐ Will Phone Again ☐ Rush

MESSAGE And mowable areas raised

Concrete around them as had

Please check & let her know

work is going to be done about

X

Message taken by

PC

11-22-93 10<sup>50</sup> am

Returned call - spoke to

Mrs. Armour.

Advised her that the street

is private & the township has

no jurisdiction or authority.

She said the owner refuses

to repair & maintain.

I told her it is a civil

matter.

NR

I will confirm 12/301

301 concurred

11-22-93

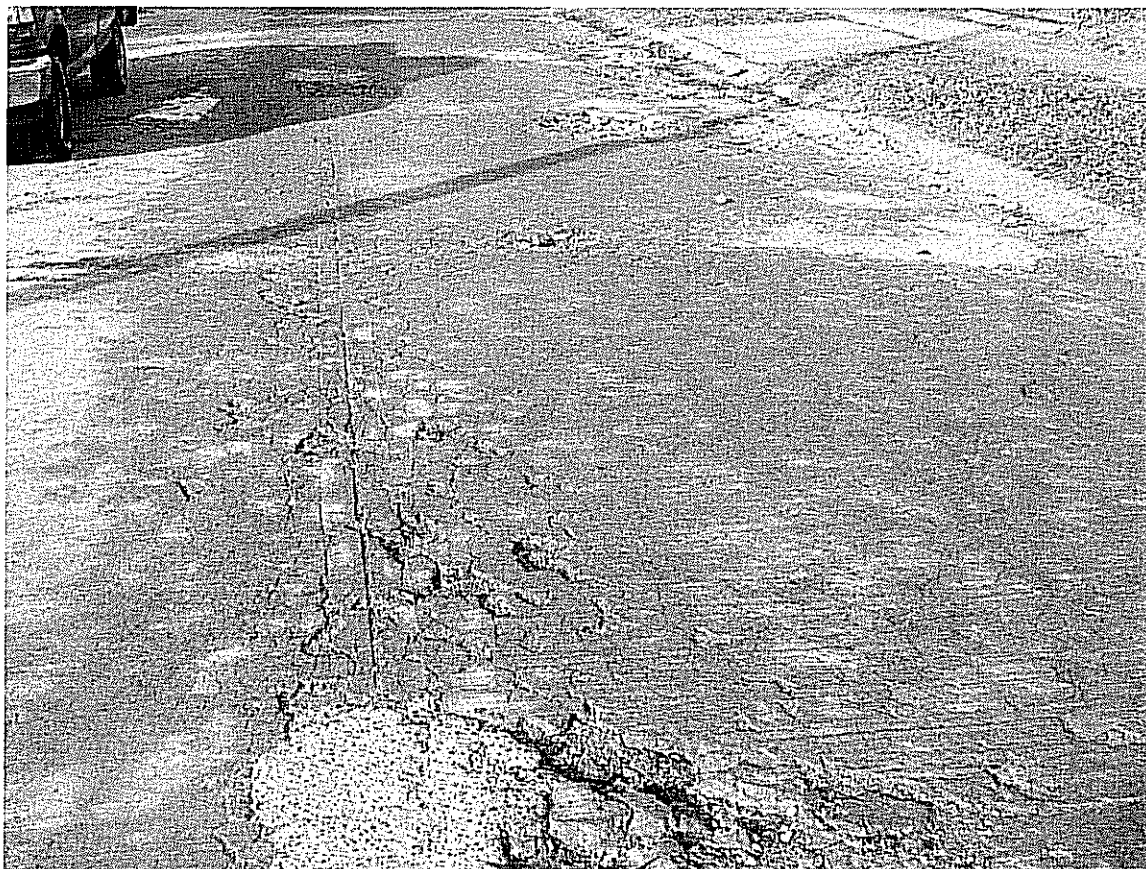
PC

# BELHAVEN DRIVE AND FLAMINGO LANE





# BELHAVEN DRIVE AND FLAMINGO LANE



# BELHAVEN DRIVE AND FLAMINGO LANE





# BELHAVEN DRIVE AND FLAMINGO LANE





# BELHAVEN DRIVE AND FLAMINGO LANE

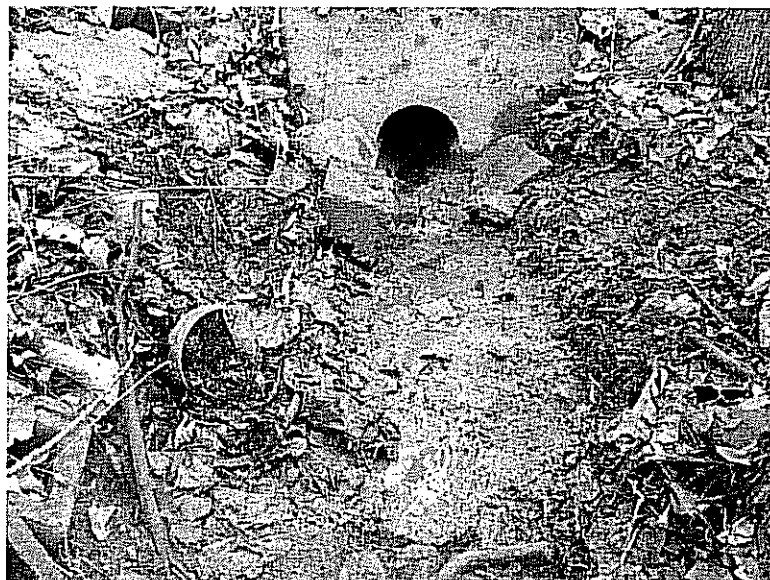


# BELHAVEN DRIVE AND FLAMINGO LANE

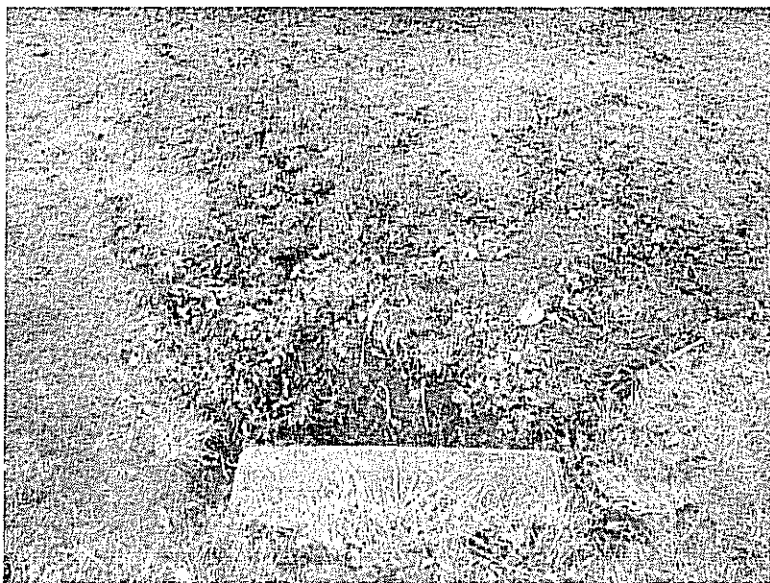




# BELHAVEN HEADWALL 2-8-08



# FLAMINGO HEADWALL 2-8-08





# BELHAVEN DRIVE AND FLAMINGO LANE



# BELHAVEN DRIVE AND FLAMINGO LANE



# ADDITIONAL SUPPORT INFORMATION

For Program Year 2009 (July 1, 2009 through June 30, 2010), applying agencies shall provide the following support information to help determine which projects will be funded. Information on this form must be accurate, and where called for, based on sound engineering principles. Documentation to substantiate the individual items, as noted, is required. The applicant should also use the rating system and its' addendum as a guide. The examples listed in this addendum are not a complete list, but only a small sampling of situations that may be relevant to a given project.

**IF YOU ARE APPLYING FOR A GRANT, WILL YOU BE WILLING TO ACCEPT A LOAN IF ASKED BY THE DISTRICT? \_\_\_\_\_YES \_\_\_\_\_X\_\_\_\_\_NO (ANSWER REQUIRED)**

Note: Answering "Yes" will not increase your score and answering "NO" will not decrease your score.

**1) What is the physical condition of the existing infrastructure that is to be replaced or repaired?**

Give a statement of the nature of the deficient conditions of the present facility exclusive of capacity, serviceability, health and/or safety issues. If known, give the approximate age of the infrastructure to be replaced, repaired, or expanded. Use documentation (if possible) to support your statement. Documentation may include (but is not limited to): ODOT BR86 reports, pavement management condition reports, televised underground system reports, age inventory reports, maintenance records, etc., and will only be considered if included in the original application.

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**See Attachment "B"**

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**2) How important is the project to the safety of the Public and the citizens of the District and/or service area?**

Give a statement of the projects effect on the safety of the service area. The design of the project is intended to reduce existing accident rate, promote safer conditions, and reduce the danger of risk, liability or injury. (Typical examples may include the effects of the completed project on accident rates, emergency response time, fire protection, and highway capacity.) Please be specific and provide documentation if necessary to substantiate the data. The applicant must demonstrate the type of problems that exist, the frequency and severity of the problems and the method of correction.

---

**See Attachment "C"**

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**3) How important is the project to the health of the Public and the citizens of the District and/or service area?**

Give a statement of the projects effect on the health of the service area. The design of the project will improve the overall condition of the facility so as to reduce or eliminate potential for disease, or correct concerns regarding the environmental health of the area. (Typical examples may include the effects of the completed project by improving or adding storm drainage or sanitary facilities, etc.). Please be specific and provide documentation if necessary to substantiate the data. The applying agency must demonstrate the type of problems that exist, the frequency and severity of the problems and the method of correction.

---

**See Attachment "D"**

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4) Does the project help meet the infrastructure repair and replacement needs of the applying jurisdiction?

The applying agency must submit a listing in priority order of the projects for which it is applying. Points will be awarded on the basis of most to least importance.

Priority 1 Belhaven Drive and Flamingo Lane Reconstruction

Priority 2 Geraldine Drive Reconstruction

Priority 3 \_\_\_\_\_

Priority 4 \_\_\_\_\_

Priority 5 \_\_\_\_\_

5) To what extent will the user fee funded agency be participating in the funding of the project?

(example: rates for water or sewer, frontage assessments, etc.).

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6) Economic Growth – How will the completed project enhance economic growth

Give a statement of the projects effect on the economic growth of the service area (be specific).

The project will not impact business development

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7) Matching Funds - LOCAL

The information regarding local matching funds is to be filed by the applying agency in Section 1.2 (b) of the Ohio Public Works Association's "Application For Financial Assistance" form.

8) Matching Funds - OTHER

The information regarding local matching funds is to be filed by the applying agency in Section 1.2 (c) of the Ohio Public Works Association's "Application For Financial Assistance" form. If MRF funds are being used for matching funds, the MRF application must have been filed by Friday, August 29, 2008 for this project with the Hamilton County Engineer's Office. List below all "other" funding the source(s).

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9) Will the project alleviate serious capacity problems or respond to the future level of service needs of the district?

Describe how the proposed project will alleviate serious capacity problems (be specific).

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Level of Service (LOS) calculations shall be for the improvements being made in the application. If this project is a phase of a larger project then any preceding phases shall be considered existing conditions for LOS calculations. Any future project phases shall not be considered as part of this applications LOS calculations.

For roadway betterment projects, provide the existing and proposed Level of Service (LOS) of the facility using the methodology outlined within AASHTO'S "Geometric Design of Highways and Streets" and the current edition of the Highway Capacity Manual.

<u>No Build</u>	<u>Proposed Geometry</u>
Current Year LOS ____	Current Year LOS ____
Design Year LOS ____	Design Year LOS ____

If the proposed design year LOS is not "C" or better, explain why LOS "C" cannot be achieved.

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10) If SCIP/LTIP funds were granted, when would the construction contract be awarded?

If SCIP/LTIP funds are awarded, how soon after receiving the Project Agreement from OPWC (tentatively set for July 1 of the year following the deadline for applications) would the project be under contract? The Support Staff will review status reports of previous projects to help judge the accuracy of a jurisdiction's anticipated project schedule.

Number of months 5

a.) Are preliminary plans or engineering completed?	Yes _____	No <u>X</u>	N/A _____
b.) Are detailed construction plans completed?	Yes _____	No <u>X</u>	N/A _____
c.) Are all utility coordination's completed?	Yes _____	No _____	N/A <u>X</u>
d.) Are all right-of-way and easements acquired (if applicable)?	Yes _____	No _____	N/A <u>X</u>

If no, how many parcels needed for project? \_\_\_\_\_ Of these, how many are: Takes \_\_\_\_\_  
Temporary \_\_\_\_\_  
Permanent \_\_\_\_\_

For any parcels not yet acquired, explain the status of the ROW acquisition process for this project.

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e.) Give an estimate of time needed to complete any item above not yet completed. 9 Months.

**11) Does the infrastructure have regional impact?**

Give a brief statement concerning the regional significance of the infrastructure to be replaced, repaired, or expanded.

The regional significance of the infrastructure being replaced is of minimal impact. These are subdivision streets that provides access to abutting properties. The project will improve the well being of the subdivision, improving the quality, structure and soundness of this street and overall enhance the area.

**12) What is the overall economic health of the jurisdiction?**

The District 2 Integrating Committee predetermines the jurisdiction's economic health. The economic health of a jurisdiction may periodically be adjusted when census and other budgetary data are updated.

**13) Has any formal action by a federal, state, or local government agency resulted in a partial or complete ban of the usage or expansion of the usage for the involved infrastructure?**

Describe what formal action has been taken which resulted in a ban of the use of or expansion of use for the involved infrastructure? Typical examples include weight limits, truck restrictions, and moratoriums or limitations on issuance of building permits, etc. The ban must have been caused by a structural or operational problem to be considered valid. Submission of a copy of the approved legislation would be helpful.

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Will the ban be removed after the project is completed? Yes \_\_\_\_\_ No \_\_\_\_\_ N/A \_\_\_\_\_

**14) What is the total number of existing daily users that will benefit as a result of the proposed project?**

For roads and bridges, multiply current Average Daily Traffic (ADT) by 1.20. For inclusion of public transit, submit documentation substantiating the count. Where the facility currently has any restrictions or is partially closed, use documented traffic counts prior to the restriction. For storm sewers, sanitary sewers, water lines, and other related facilities, multiply the number of households in the service area by 4. User information must be documented and certified by a professional engineer or the jurisdictions' C.E.O.

Traffic: ADT 125 X 1.20 = 150 Users

Water/Sewer: Homes \_\_\_\_\_ X 4.00 = \_\_\_\_\_ Users

**15) Has the jurisdiction enacted the optional \$5 license plate fee, an infrastructure levy, a user fee, or dedicated tax for the pertinent infrastructure?**

The applying jurisdiction shall list what type of fees, levies or taxes they have dedicated toward the type of infrastructure being applied for. (Check all that apply)

Optional \$5.00 License Tax X

Infrastructure Levy \_\_\_\_\_ Specify type \_\_\_\_\_

Facility Users Fee \_\_\_\_\_ Specify type \_\_\_\_\_

Dedicated Tax \_\_\_\_\_ Specify type \_\_\_\_\_

Other Fee, Levy or Tax \_\_\_\_\_ Specify type \_\_\_\_\_

ATTACHMENT "A"

GERALDINE DRIVE RECONSTRUCTION

<u>STREET</u>	<u>LIMITS</u>		<u>LENGTH</u>
	<u>FROM</u>	<u>TO</u>	
Belhaven Drive	Haskell Drive	Culdesac	409'
Flamingo Lane	Belhaven Drive	End	325'
<b>TOTAL</b>			<b>734'</b>

## ATTACHMENT "B"

### **A Description and Condition of Belhaven Drive and Flamingo Lane**

Belhaven Drive and Flamingo Lane were built around 1959; the streets were never accepted as township streets until 2007 and during that time had little to no maintenance done to the street - See Supporting documents.

The streets in this application are concrete streets 25 feet back to back of curb. The streets are 49 years old. The streets have a high deterioration rate and suffer from numerous load and climate related distresses. Rideability is poor because of the heaved joints. After the streets were accepted by Hamilton County and Colerain Township we did some patching, leveling, tree trimming and removal and opened the headwalls to improve drainage and improve conditions until the streets can be reconstructed - see attached pictures. The existing base and subbase has failed and is voided throughout. There are uneven and faulted slabs, slabs with high manholes, voided pavement, sinkholes deteriorated curbs, potholes and patches, poor drainage and overall weathering and raveling of the pavement, - see attached pictures. There are several storm lines in need of repair and replacement. There are numerous problems with them such as open off set joints, cracked and broken pipes, infiltration of joints, etc. - see attached TV reports. Colerain Township's pavement management program, Micro Paver rates this street with a high deterioration rate with a serious to failed PCI condition rating - see attached inspection reports. From our recent inspection for this application, these streets are in need of reconstruction.

## ATTACHMENT "C"

### **This Project Is Important To The Safety Of The Public And Residents**

The existing pavement is rough and bumpy which makes the rideability poor and increases the chance for accidents. The existing sidewalks are faulted and deteriorated making for unsafe conditions. There are no ADA ramps either. This situation is worsened when the pavement is wet, especially when the standing water freezes in the winter. The elimination of the standing water should lessen the chance of accidents occurring. The smoother pavement will improve the rideability and snow and ice removal efforts should be more effective.

Until this year these residents have never seen any improvements or maintenance on these streets. They will welcome and take additional pride in their street and make improvements to their private properties thus enhancing the overall safety of the area

These are factors that impact the safety of the service area. The reconstruction project should improve vehicular and pedestrian safety by promoting safer conditions. The installation of items such as the new concrete curbs, asphalt pavement, catch basin, new sidewalks and storm lines should correct the many problems of this street.

## ATTACHMENT "D"

### **This Project Is Important To The Health Of The Public And Residents**

This project will improve the overall condition of the street so as to reduce or eliminate potential for disease and correct concerns regarding the environmental health of the street.

There are storm lines on this street in need of repair and replacement. We have included our TV reports of the existing storm lines condition.

This street has areas of standing water on it due to the uneven blocks, patches, bumps and sags etc. There are storm lines that have bellies, offset joints, dropped sections, etc. which also hold water. All of the above items can lead to serious health problems. The water described above will produce a smell and can carry bacteria's and other diseases, that kids, animals etc. can come in contact with.

We listed storm lines and catch basins to be replaced and underdrains to be added on the project components section of the application as a method of correction and on the engineer's estimate. These components will work hand-in-hand to improve the overall condition and health of the area and these problems will be improved only with the reconstruction project.

**SCIP/LTIP PROGRAM  
ROUND 23 - PROGRAM YEAR 2009  
PROJECT SELECTION CRITERIA  
JULY 1, 2009 TO JUNE 30, 2010**

NAME OF APPLICANT: COLERAIN TOWNSHIP

NAME OF PROJECT: BEILHAUSE DRIVE AND FENNINGS LANE RECONSTR

RATING TEAM: 4

**General Statement for Rating Criteria**

Points awarded for all items will be based on engineering experience, field verification, application information and other information supplied by the applying agency, which is deemed to be relevant by the Support Staff. The examples listed in this addendum are not a complete list, but only a small sampling of situations that may be relevant to a given project.

**CIRCLE THE APPROPRIATE RATING**

1) What is the physical condition of the existing infrastructure that is to be replaced or repaired?

25 - Failed

23 - Critical

20 - Very Poor

17 - Poor

15 - Moderately Poor

10 - Moderately Fair

5 - Fair Condition

0 - Good or Better

Appeal Score

\_\_\_\_\_

**Criterion 1 - Condition**

Condition of the particular infrastructure to be repaired, reconstructed or replaced shall be a measure of the degree of reduction in condition from its original state. Historic pavement management data based on ASTM D6433-99 rating system may be submitted as documentation. Capacity, serviceability, safety and health shall not be considered in this criterion. Any documentation the Applicant wishes to be considered must be included in the application package.

**Definitions:**

**Failed Condition** - requires complete reconstruction where no part of the existing facility is salvageable. (E.g. Roads: complete reconstruction of roadway, curbs and base; Bridges: complete removal and replacement of bridge; Underground: removal and replacement of an underground drainage or water system.

**Critical Condition** - requires partial reconstruction to maintain integrity. (E.g. Roads: reconstruction of roadway/curbs can be saved; Bridges: removal and replacement of bridge with abutment modification; Underground: removal and replacement of part of an underground drainage or water system.

**Very Poor Condition** - requires extensive rehabilitation to maintain integrity. (E.g. Roads: extensive full depth, partial depth and curb repair of a roadway with a structural overlay; Bridges: superstructure replacement; Underground: repair of joints and/or replacement of pipe sections.

**Poor Condition** - requires standard rehabilitation to maintain integrity. (E.g. Roads: moderate full depth, partial depth and curb repair to a roadway with no structural overlay needed or structural overlay with minor repairs to a roadway needed; Bridges: extensive patching of substructure and replacement of deck; Underground: insituform or other in ground repairs.

**Moderately Poor Condition** - requires minor rehabilitation to maintain integrity. (E.g. Roads: minor full depth, partial depth or curb repairs to a roadway with either a thin overlay or no overlay needed; Bridges: major structural patching and/or major deck repair.

**Moderately Fair Condition** - requires extensive maintenance to maintain integrity. (E.g. Roads: thin or no overlay with extensive crack sealing, minor partial depth and/or slurry or rejuvenation; Bridges: minor structural patching, deck repair, erosion control.)

**Fair Condition** - requires routine maintenance to maintain integrity. (E.g. Roads: slurry seal, rejuvenation or routine crack sealing to the roadway; Bridges: minor structural patching.)

**Good or Better Condition** - little to no maintenance required to maintain integrity.

**Note:** If the infrastructure is in "good" or better condition, it will **NOT** be considered for SCIP/LTIP funding unless it is an expansion project that will improve serviceability.

2) How important is the project to the safety of the Public and the citizens of the District and/or service area?

25 - Highly significant importance

20 - Considerably significant importance

15 - Moderate importance

10 - Minimal importance

5 - Poorly documented importance

0 - No measurable impact

Appeal Score

\_\_\_\_\_

**Criterion 2 – Safety**

The applying agency shall include in its application the type of deficiency that currently exists and how the intended project would improve the situation. For example, have there been vehicular accidents attributable to the problems cited? Have they involved injuries or fatalities? In the case of water systems, are existing hydrants non-functional? In the case of water lines, is the present capacity inadequate to provide volumes or pressure for adequate fire protection? **In all cases, specific documentation is required.** Mentioned problems, which are poorly documented, generally will not receive more than 5 points.

**Note:** Each project is looked at on an individual basis to determine if any aspects of this category apply. Examples given above are **NOT** intended to be exclusive.

3) How important is the project to the health of the Public and the citizens of the District and/or service area?

25 - Highly significant importance

20 - Considerably significant importance

15 - Moderate importance

10 - Minimal importance

5 - Poorly documented importance

0 - No measurable impact

Appeal Score

\_\_\_\_\_

**Criterion 3 – Health**

The applying agency shall include in its application the type, frequency, and severity of the health problem that would be eliminated or reduced by the intended project. For example, can the problem be eliminated only by the project, or would routine maintenance be satisfactory? If basement flooding has occurred, was it storm water or sanitary flow? What complaints if any are recorded? In the case of underground improvements, how will they improve health if they are storm sewers? How would improved sanitary sewers improve health or reduce health risk? **In all cases, quantified documentation is required.** Mentioned problems, which are poorly documented, generally will not receive more than 5 points.

**Note:** Each project is looked at on an individual basis to determine if any aspects of this category apply. Examples given above are **NOT** intended to be exclusive.

4) Does the project help meet the infrastructure repair and replacement needs of the applying agency?

Note: Applying agency's priority listing (part of the Additional Support Information) must be filed with application(s).

25 - First priority project

20 - Second priority project

15 - Third priority project

10 - Fourth priority project

5 - Fifth priority project or lower

Appeal Score

\_\_\_\_\_

**Criterion 4 – Jurisdiction's Priority Listing**

The applying agency **must** submit a listing in priority order of the projects for which it is applying. Points will be awarded on the basis of most to least importance. The form is included in the Additional Support Information.



- 5) To what extent will a user fee funded agency be participating in the funding of the project?
- 10 - Less than 10%
  - 9 - 10% to 19.99%
  - 8 - 20% to 29.99%
  - 7 - 30% to 39.99%
  - 6 - 40% to 49.99%
  - 5 - 50% to 59.99%
  - 4 - 60% to 69.99%
  - 3 - 70% to 79.99%
  - 2 - 80% to 89.99%
  - 1 - 90% to 95%
  - 0 - Above 95%

Appeal Score  
\_\_\_\_\_

**Criterion 5 – User Fee-funded Agency Participation**

To what extent will a user fee funded agency be participating in the funding of the project? (Example: rates for water or sewer, frontage assessments, etc.). The applying agency must submit documentation.

- 6) Economic Growth – How the completed project will enhance economic growth (See definitions).

- 10 – The project will directly secure new employment
- 5 – The project will permit more development
- 0 – The project will not impact development

Appeal Score  
\_\_\_\_\_

**Criterion 6 – Economic Growth**

Will the completed project enhance economic growth and/or development ~~within the service area?~~

**Definitions:**

**Secure new employment:** The project as designed will secure development/employers, which will immediately add new permanent employees ~~to the jurisdiction~~. The applying agency must submit details.

**Permit more development:** The project as designed will permit additional business development/employment. The applying agency must supply details.

**The project will not impact development:** The project will have no impact on business development.

**Note:** Each project is looked at on an individual basis to determine if any aspects of this category apply.

- 7) Matching Funds - **LOCAL**

- 10 - This project is a loan or credit enhancement
- 10 - 50% or higher
- 8 - 40% to 49.99%
- 6 - 30% to 39.99%
- 4 - 20% to 29.99%
- 2 - 10% to 19.99%
- 0 - Less than 10%

List total percentage of "Local" funds 30 %

**Criterion 7 – Matching Funds – Local**

The percentage of matching funds which come directly from the budget of the applying agency. Ten points shall be awarded if a loan request is at least 50% of the total project cost. (If the applying agency is not a user fee funded agency, any funds to be provided by a user fee generating agency will be considered "Matching Funds – Other").

8) Matching Funds – OTHER List total percentage of “Other” funds 0 %

- 10 – 50% or higher
- 8 – 40% to 49.99%
- 6 – 30% to 39.99%
- 4 – 20% to 29.99%
- 2 – 10% to 19.99%
- 1 – 1% to 9.99%
- 0 – Less than 1%

List below each funding source and percentage

_____	_____ %
_____	_____ %
_____	_____ %
_____	_____ %
_____	_____ %

**Criterion 8 – Matching Funds - Other**  
The percentage of matching funds that come from funding sources other than those mentioned in Criterion 7. A letter from the outside funding agency stating their financial participation in the project and the amount of funding is required to receive points. For MRF, a copy of the current application form filed with the Hamilton County Engineer’s Office meets the requirement.

9) Will the project alleviate serious capacity problems or hazards or respond to the future level of service needs of the district?

- 10 - Project design is for future demand.
  - 8 - Project design is for partial future demand.
  - 6 - Project design is for current demand.
  - 4 - Project design is for minimal increase in capacity.
  - 0 - Project design is for no increase in capacity.
- Appeal Score  
\_\_\_\_\_

**Criterion 9 – Alleviate Capacity Problems**  
The applying agency shall provide a narrative, along with pertinent support documentation, which describe the existing deficiencies and showing how congestion will be reduced or eliminated and how service will be improved to meet the needs of any expected growth or development. A formal capacity analysis must accompany the application to receive more than 4 points. Projected traffic or demand should be calculated as follows:

**Formula:**  
Existing volume x design year factor = projected volume

Design Year	Design year factor		
	Urban	Suburban	Rural
20	1.40	1.70	1.60
10	1.20	1.35	1.30

- Definitions:**
- Future demand** – Project will eliminate existing congestion or deficiencies and will provide sufficient capacity or service for twenty-year projected demand or fully developed area conditions. Justification must be supplied if the area is already largely developed or undevelopable and thus the projection factors used deviate from the above table.
  - Partial future demand** – Project will eliminate existing congestion or deficiencies and will provide sufficient capacity or service for ten-year projected demand or partially developed area conditions. Justification must be supplied if the area is already largely developed or undevelopable and thus the projection factors used deviate from the above table.
  - Current demand** – Project will eliminate existing congestion or deficiencies and will provide sufficient capacity or service only for existing demand and conditions.
  - Minimal increase** – Project will reduce but not eliminate existing congestion or deficiencies and will provide a minimal but less than sufficient increase in existing capacity or service for existing demand and conditions.
  - No increase** – Project will have no effect on existing congestion or deficiencies and provide no increase in capacity or service for existing demand and conditions.

10) Readiness to Proceed - If SCIP/LTIP funds are granted, when would the construction contract be awarded?

- 5 - Will be under contract by December 31, 2009 and no delinquent projects in Rounds 20 & 21
- 3 - Will be under contract by March 31, 2010 and/or one delinquent project in Rounds 20 & 21
- 0 - Will not be under contract by March 31, 2010 and/or more than one delinquent project in Rounds 20 & 21

Criterion 10 – Readiness to Proceed

The Support Staff will assign points based on engineering experience and status of design plans. A project is considered delinquent when it has not received a notice to proceed within the time stated on the original application and no time extension has been granted by the OPWC. An applying agency receiving approval for a project and subsequently canceling the same after the bid date on the application will receive zero (0) points under this round and the following round.

11) Does the infrastructure have regional impact? Consider origination and destination of traffic, functional classifications, size of service area, and number of jurisdictions served, etc.

10 – Major Impact	Appeal Score
8 – Significant Impact	
6 – Moderate Impact	
4 – Minor Impact	
2 – Minimal or No Impact	

Criterion 11 - Regional Impact

The regional significance of the infrastructure that is being repaired or replaced.

Definitions:

**Major Impact** – Roads: Major Arterial: A direct connector to an Interstate Highway; Arterials are intended to provide a greater degree of mobility rather than land access. Arterials generally convey large traffic volumes for distances greater than one mile. A major arterial is a highway that is of regional importance and is intended to serve beyond the county. It may connect urban centers with one another and/or with outlying communities and employment or shopping centers. A major arterial is intended primarily to serve through traffic.

**Significant Impact** – Roads: Minor Arterial: A roadway, also serving through traffic, that is similar in function to a major arterial, but operates with lower traffic volumes, serves trips of shorter distances (but still greater than one mile), and may provide a higher degree of property access than do major arterials.

**Moderate Impact** – Roads: Major Collector: A roadway that provides for traffic movement between local roads/streets and arterials or community-wide activity centers and carries moderate traffic volumes over moderate distances (generally less than one mile). Major collectors may also provide direct access to abutting properties, such as regional shopping centers, large industrial parks, major subdivisions and community-wide recreational facilities, but typically not individual residences. Most major collectors are also county roads and are therefore through streets.

**Minor Impact** – Roads: Minor Collector: A roadway similar in functions to a major collector but which carries lower traffic volumes over shorter distances and has a higher degree of property access. Minor collectors may serve as main circulation streets within large, residential neighborhoods. Most minor collectors are also township roads and streets and may, or may not, be through streets.

**Minimal or No Impact** – Roads: Local: A roadway that is primarily intended to provide access to abutting properties. It tends to accommodate lower traffic volumes, serves short trips (generally within neighborhoods), and provides connections preferably only to collector streets rather than arterials.

12) What is the overall economic health of the jurisdiction?

- 10 Points
- 8 Points
- 6 Points
- 4 Points
- 2 Points

**Criterion 12 – Economic Health**  
The District 2 Integrating Committee predetermines the applying agency’s economic health. The economic health of a jurisdiction may periodically be adjusted when census and other budgetary data are updated.

13) Has any formal action by a federal, state, or local government agency resulted in a partial or complete ban of the usage or expansion of the usage for the involved infrastructure?

- |   |              |
|---|--------------|
| 10 - Complete ban, facility closed  | Appeal Score |
| 8 – 80% reduction in legal load or 4-wheeled vehicles only                      |              |
| 7 – Moratorium on future development, <i>not</i> functioning for current demand | _____        |
| 6 – 60% reduction in legal load   |              |
| 5 - Moratorium on future development, functioning for current demand            |              |
| 4 – 40% reduction in legal load   |              |
| 2 – 20% reduction in legal load   |              |
| 0 - Less than 20% reduction in legal load                                       |              |

**Criterion 13 - Ban**  
The applying agency shall provide documentation to show that a facility ban or moratorium has been formally placed. The ban or moratorium must have been caused by a structural or operational problem. Points will only be awarded if the end result of the project will cause the ban to be lifted.

14) What is the total number of existing daily users that will benefit as a result of the proposed project?

- |                      |              |
|----------------------|--------------|
| 10 - 30,000 or more  | Appeal Score |
| 8 - 21,000 to 29,999 |              |
| 6 - 12,000 to 20,999 | _____        |
| 4 - 3,000 to 11,999  |              |
| 2 - 2,999 and under  |              |

**Criterion 14 - Users**  
The applying agency shall provide documentation. A registered professional engineer or the applying agency’s C.E.O must certify the appropriate documentation. Documentation may include current traffic counts, households served, when converted to a measurement of persons. Public transit users are permitted to be counted for the roads and bridges, but only when certifiable ridership figures are provided.

15) Has the applying agency enacted the optional \$5 license plate fee, an infrastructure levy, a user fee, or dedicated tax for the pertinent infrastructure? *(Provide documentation of which fees have been enacted.)*

- |                              |              |
|------------------------------|--------------|
| 5 - Two or more of the above | Appeal Score |
| 3 - One of the above         |              |
| 0 - None of the above        | _____        |

**Criterion 15 – Fees, Levies, Etc.**  
The applying agency shall document (in the “Additional Support Information” form) which type of fees, levies or taxes they have dedicated toward the type of infrastructure being applied for.